



# George Street Masterplan Finalised Draft



11th October 2023



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View along George Street  
looking towards Aberdeen  
City Centre



# **1. Introduction & Context For Change**

# 1. Introduction & Context For Change

## Role Of The Masterplan

This Masterplan has been prepared on behalf of Aberdeen City Council by Optimised Environments Ltd., with support from Streets UK and Systra.

**The purpose of the Masterplan is to establish a clear Vision, supporting objectives and set of development principles. Founded on engagement and endorsement by the communities of the George Street area, the Masterplan is to stimulate and support the delivery of incremental, transformational change by securing a range of economic (economy), social (people) and environmental (place) benefits.**

Delivery of the Masterplan will help to ensure that George Street builds upon the successful and vibrant community by supporting and identifying areas for positive change and investment within the city centre boundary.

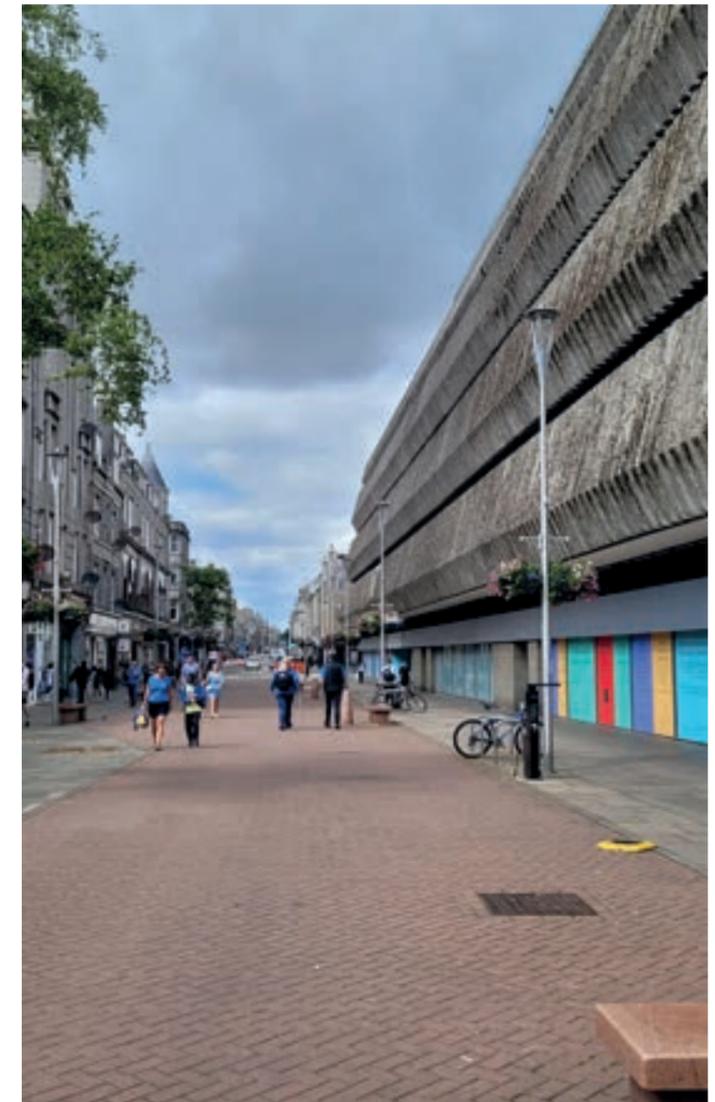
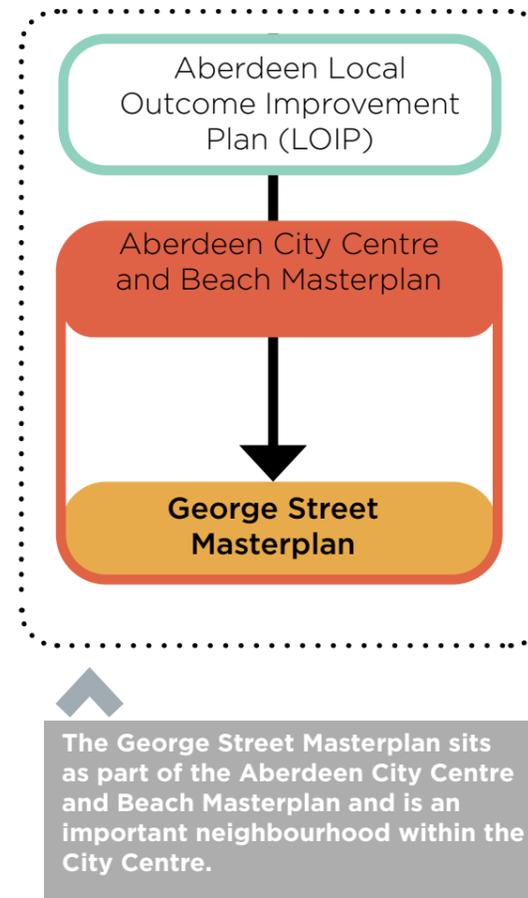
The overall objective of the Masterplan for the George Street area is to support the longer term success of the neighbourhood as the Council's Strategy for Change.

This Masterplan forms part of a wider City Centre and Beach Masterplan which will be regularly reviewed to ensure Aberdeen City Council's strategy remains up to date and relevant, with a clear focus on delivery.

The proposals put forward in this Masterplan build upon, support and encourage activity that is already taking place on the ground. They vary from the small to the large-scale, from quick wins to long-term aims for growth. It includes recommendations that range from public realm enhancements to business support, from wayfinding interventions to meanwhile uses in underused premises.

The George Street Masterplan has been designed as a public tool to be used by local residents, community groups, public authorities and other stakeholders to take ownership and co-deliver projects. It is hoped that complementary projects can also be considered for the wider George Street neighbourhood.

The potential projects have been developed based on feedback from the community and local stakeholders and in combination they form a holistic strategy and Vision for George Street.



View north along George Street with Norco House to the right

# Welcome to George Street

Located to the north of the city centre, George Street is a historic urban neighbourhood which has retained its distinctive character. Today it is a bustling neighbourhood, where a variety of independent retail businesses sit amongst residential tenements, education institutions and leisure uses, all set out within a historic street grid.

The neighbourhood's look and feel is characterised by a number of attractive historic buildings, an example of brutalist architecture and the vibrant and vivid art murals scattered across the area and wider city as part of the NuArt festival.

## History

The neighbourhood grew as part of Aberdeen's outward growth from Union Street and by 1828 much of the southern part of the neighbourhood was established. The area was historically directly connected to Aberdeen's city centre, Union Street and the harbour. Its tramway which ran parallel to King Street, formed a direct link between the city centre and fringe market areas such as Kittybrewster. As a result of its proximity to the city centre and the variety of people and amenities needed to support Aberdeen's growth, the neighbourhood developed as a dense urban area comprising of Candle & Soap Works, Public Baths, Foundries and terraced streets of worker's housing.

Although much of that historic street network and built form remains today, there have been some significant changes to its historic character.

Built during 1966-1970 along George Street, Norco House was built for the Northern Co-operative Society. This brutalist, ziggurat (stepped) 4 storey building presents a striking contrast to the smaller scaled, traditional stone buildings.

Both the George Street and Loch Street connection to Union Street was severed in c.1990-1995 with the development of the Bon Accord Centre which dramatically impacted upon people's ability to access Union Street directly.

## George Street Today

The neighbourhood today continues as a mixed use, urban high street. The granite tenement buildings remain with local retail shops at the ground floor, complemented by flatted residential dwellings on the upper floors.

George Street continues to play an important role within the wider city centre in terms of residential accommodation. The neighbourhood offers people the opportunity to access more affordable and attainable residential accommodation within a city centre setting.

The retail offer across the neighbourhood is focused along George Street and has historically been split into two. To the south along George Street the Bon Accord Centre and former John Lewis has attracted larger retailers such as Blacks, Magnet Kitchens and Greggs. To the north of George Street, the majority of retail units comprise of a variety of local, independent health and beauty shops, takeaways and repair shops, creating a vitality to the area and which drive most of the day time activity in the area.

North East Scotland College (NESCOL) within the neighbourhood and Robert Gordon's College to the south west are important local institutions that interact with George Street.

With that said, there are a number of challenges that the George Street area faces that may impact the future prosperity of the neighbourhood unless addressed. These include:

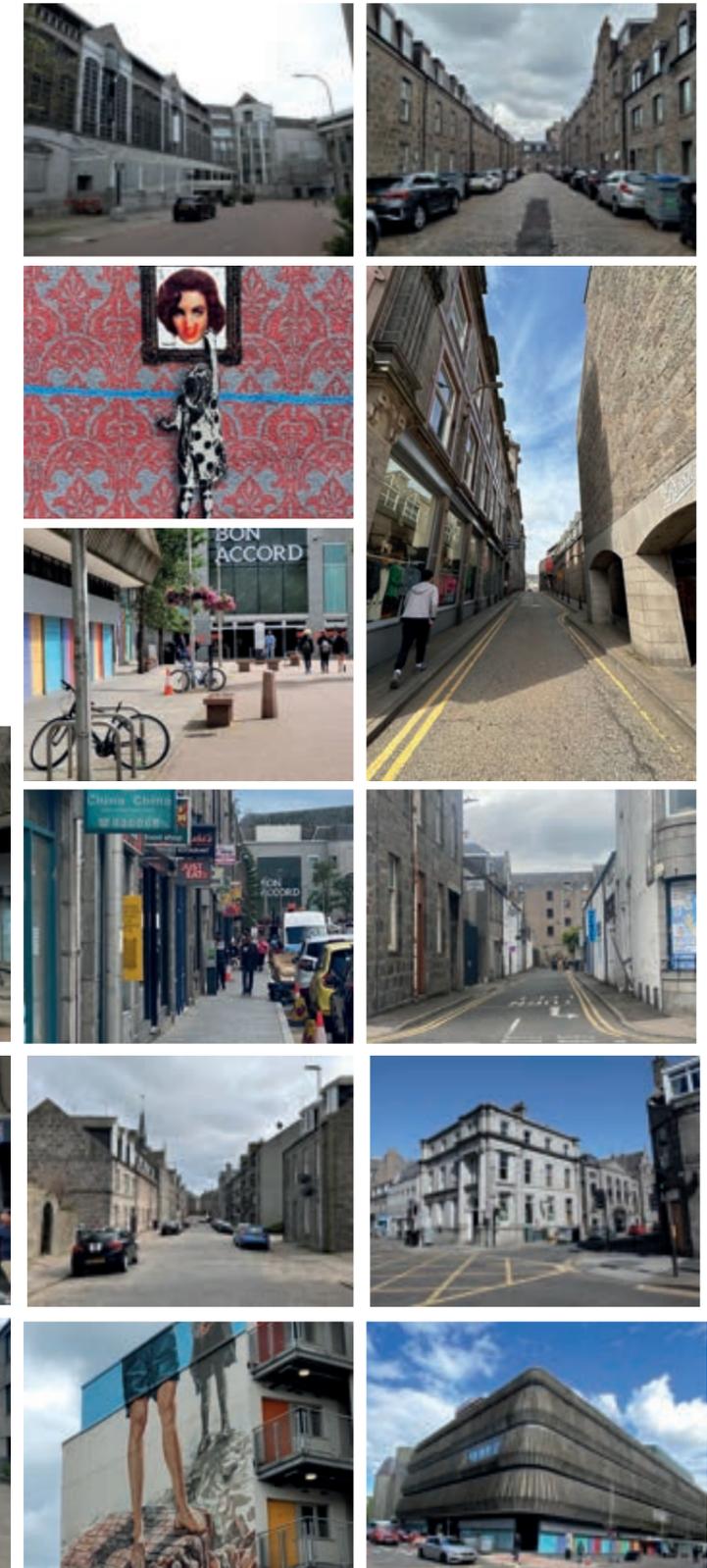
- Norco House remains vacant but in a prime location within the neighbourhood
- Broadford Works remains undeveloped and could be a catalyst for further development within the area
- High Streets are struggling across the UK and George Street is no different, where appropriate, financial support and investment would be welcome

- The George Street neighbourhood needs to strengthen its pull to visitors promote the creation of destination points
- The area is dominated by vehicles and in the midst of the Climate Crisis, the area should consider more sustainable modes of transport and active travel routes.

It is hoped that the George Street Masterplan will provide solutions to these challenges and revitalise the wider neighbourhood.

The collection of images below give a sense of the variety, history and present day character of the neighbourhood, both along George Street and across the area's historic streets.

The collage of site photos convey the variety of environments across the Masterplan area



## Extent of George Street Masterplan

The area identified for the Masterplan is outlined opposite in red. The George Street neighbourhood is located towards the northern boundary of the City Centre & Beach Masterplan area. George Street itself runs North to South, forming a once historic key route between the city centre core and areas to the North of the city.

The area is defined to the south by the Bon Accord Centre, which severs the historic connection to the City Centre. It is bounded to the south west by Robert Gordon College, to the east and northeast by Gallowgate and North East Scotland College (NESCOL), and to the north by Spring Garden.

The focus of the Masterplan is primarily within the study area, however, in order for wider benefits to be leveraged from investment within the George Street area, the Masterplan must acknowledge and respond to opportunities beyond its boundaries, understanding the influence of planned investment / development.

With a thriving business economy, Aberdeen is in a prime location spatially and economically for future development. Further information on the strategic context can be found within the Aberdeen City Centre and Beach Masterplans.



Figure 1 - George Street in Context

## Collaboration

The Masterplan has been developed alongside engagement with statutory stakeholders, community stakeholders and any interested party. Each of the key observations have been recorded, reviewed and integrated as the Masterplan has been developed.

The following have influenced the direction of the Masterplan. Other events and specific engagement has been carried out to support the Masterplan, which are summarised in the Engagement Document:

- Public consultation held in February 2022
- Strategic Environmental Assessment (SEA) Environmental report April and August 2023
- Public consultation held in March and April 2023

The following pages provides a brief summary of the key findings of each of the above engagement processes.

## SEA Environmental Report

Under the Environmental Assessment (Scotland) Act 2005 a SEA was considered to be required for George Street, which was carried out alongside the initial drafting of the George Street Masterplan.

The SEA process seeks to ensure that the Masterplan, once delivered, contributes positively to a high level of environmental protection across the study area and ensure that any potential significant effects are identified and alternatives explored and evaluated, where necessary, before the Masterplan is adopted.

The SEA process began in October 2022 and since then has been undertaken in parallel with the development of the draft George Street Masterplan with interaction between the SEA team and the George Street Masterplan team.

The SEA identifies 9 topics which the draft Masterplan was evaluated against. The key findings and recommendations from the SEA are set out below as a summary, with the full SEA Environmental Report appended to the Masterplan.

- 1. Biodiversity (Flora & Fauna)** - positive effects on Flora & Fauna as biodiversity is considered and promoted through several Masterplan projects. Some potential impact and options associated with future demolition of Norco House
- 2. Population & Health** - positive effects on population and health due to improved access to facilities and infrastructure. Managed vehicular traffic, sustainable transport improvements and active travel promotion combine to promote healthier lifestyles.
- 3. Water** - mixed effects on water resources. Provision of SUDS promotes biodiversity net gain. The area experiences some localised flooding associated with combined sewers. A Flood and Drainage Impact Assessment will be required prior to development.

- 4. Soil** - proposed use of SUDS and public realm improvements can help to filter out any potential historic contaminants associated with previous uses on the site.
- 5. Air** - positive effects on air quality within the Masterplan area. Design principles promote active travel and reducing vehicular travel through the Masterplan area. The Masterplan area sits to the north of the City Centre Air Quality Management Area (AQMA) and Low Emissions Zone (LEZ), and the promotion of sustainable travel aims to assist meeting City Centre air quality targets and LEZ objectives.
- 6. Climatic Factors** - positive due to pathways and actions towards Net Zero and climate resilient assets.
- 7. Cultural Heritage** - positive effects on cultural heritage. The redevelopment of Norco House (two options explored) has a mixed effect. Historic assets will be protected and enhanced, improving people's awareness and appreciation of the area's historic culture
- 8. Landscape / Streetscape** - the overall impacts on streetscape are important, principally focused around the redevelopment of Norco House and the potential impact of redevelopment on the site. The provision of new public spaces and landscape features associated with the Norco House options which bring benefits. The use of rain gardens, SUDS, green walls and bioretention verges will have a positive landscape impact.
- 9. Material assets** - the overall impact is mixed as no specific proposals were cited in the draft Masterplan. Development scenarios may involve demolition which increases waste, however proposed re-use of land / buildings has positive effects.

The George Street Masterplan has the potential to provide significant overall positive effects. The Norco House development scenarios could also be considered positive from an SEA perspective given their potential to be high quality solutions. Where there are mixed effects there are opportunities which could be positive if the process to development and intervention are comprehensively undertaken.

The increasing climate change and biodiversity crisis has influenced the Masterplan and it actively seeks to respond to the current challenges. A combination of enhancement and mitigation measures will be incorporated into the Environmental Report. Further detail would include tree planting, mini-habitats, street greening, edible and sensory plants to enhance long-term biodiversity in the area.

In accordance with the Environmental Assessment (Scotland) Act 2005, Section 16 Consultation Procedures, a Notice advertising that the Draft Environmental Report, and associated non-technical summary, was placed in the Aberdeen Evening Express newspaper on Wednesday 8th August.

The Notice advised that for a period of four weeks, between 9th August and 10th September 2023 the documents would be hosted online using the Aberdeen City Council CitizenSpace webpage and interested party opinion was sought on the content of the Draft Environmental Report. The Environmental Report illustrates what are considered to be 'significant' environmental impacts to the environment of the masterplan area, or the mitigation measures considered necessary, should the recommended projects be implemented. The consultation did not generate any responses.

## Sustainability Assessment

Sustainability is a broad and all encompassing concept but when successfully incorporated as part of any project, regardless of scale and context, it will harness and secure the future success of that project. One approach to understanding its application and relevance to George Street can broadly consist of the following five capitals (established by Forum for the Future), which include:

- **Manufactured Capital - provision of goods and services to meet human needs;**
- **Natural Capital - use of renewable materials / energy, ecological diversity, environmental capacity;**
- **Financial Capital - generating income and financially sustainable;**
- **Social Capital - community diversity, community governance and inclusivity and engagement; and**
- **Human Capital - improved quality of life, well-being, knowledge and skills as well as protecting minority interests.**

The social, economic and environmental sustainability of George Street is paramount to the Masterplan and the Transformational Projects identified help to deliver its Vision and place objectives.

The Masterplan sets out a Vision which aims to promote local enterprise, encourage social mobility and respond to the climate change crisis through environmental improvements. The balance and delivery of these will have various positive impacts for the neighbourhood as a whole.

The Masterplan sets out a best practicable sustainable solution (which is subject to further detail design testing) to meeting the objectives set out within the strategy. This would be evaluated through a Sustainability Assessment.

The Sustainability Assessment would help to define what success for George Street looks like and to ensure opportunities are being harnessed. It would apply a process which is designed to measure the impact of social, environmental and economic interventions and where the balance or emphasis across each should lie to deliver the most impactful and long term positive change for the neighbourhood.

The assessment would also recognise that any specific project has a mixture of advantages and disadvantages. As a result, the most favourable option is that which provides the most overall benefit with the fewest down sides. Understanding what is the most favourable option will be assessed against a set of agreed and weighted criteria.

## Inclusive Design

In accordance with the Equality Act 2010, which provides the legal framework protecting people with disabilities from discrimination, Aberdeen City Council requires an inclusive approach in the development of masterplans and in project delivery. It is important that the internal and external environments meet the highest levels of accessible and inclusive design, and the Masterplan starts with the simple premise that **inclusion**, rather than just **accessibility**, is embedded in the design process from the outset.

Impactful inclusive design is about people, it is about considering a range of abilities, age groups and community background. It reflects the different faiths, disabilities & hidden conditions, genders and addresses important issues that affect a neurodiverse population. People should be able to make effective, independent choices about how they use George Street without experiencing undue effort or separation and be able to participate equally in the activities the area offers. **True inclusive design benefits all of us**, it aims to remove barriers from the environment that impact not only people with protected characteristics, but others such as families with children, people carrying heavy baggage, pregnant women, people with temporary injuries and older people.

No matter how physically accessible a space is, without clear and robust management and operational policy mechanisms, the accessibility of the buildings and spaces will be compromised and over time, and diminish. Therefore, sufficiently robust provision, criterion and practices must be established by the building management and end user operators, this will include the development of Active Management Plans.

**An inclusive design approach:**

- **puts people at the heart of the design process**
- **acknowledges human diversity and difference**
- **offers dignity, autonomy, choice and spontaneity**
- **provides for flexibility in use**
- **provides buildings and environments convenient, safe and enjoyable for everyone to use**
- **goes beyond just meeting minimum standards or legislative requirements, and;**
- **recognises everyone benefits from improved accessibility.**

## What George Street Wants

### Public consultation February 2022

In February 2022 Aberdeen City Council undertook an initial online consultation to understand local stakeholder and communities views and aspirations for the George Street area.

The following provides a brief summary of that feedback which has directly informed the need for the Masterplan and the potential projects within it.

### Key messages

- Majority of 293 responses received were from people who did not live in the neighbourhood; however most of those who did comment did travel into or through the area. This means comments were based on first hand experiences of the place although not based on experiences as a resident
- Most travelled into the area for retail and leisure
- 447 comments mentioned the area's strengths with location being its greatest strength followed by its accessibility and diversity
- However around 700 comments highlighted the areas weaknesses, with poor maintenance being the most commented followed by poor condition of public realm and poor accessibility
- Similarly, over 700 comments suggested aspirations for change focused namely on economic growth, improving the public realm and improved maintenance
- Focusing on public realm improvements in particular, there was strong support for interventions such as:
  - Better cycling facilities
  - More green spaces
  - More generous pedestrian spaces
  - Introducing more colour to the streetscape
  - Creating a more welcoming street ambience
  - Updating the streetscape
  - Improve the cleanliness of the area
  - Clean and maintain building façades and;
  - Better manage traffic movement through the area.

**Consultation Feedback Quotes**

- “...I think because some of the shops leading from Union Square to George Street are closed it doesn't flow well, therefore you either don't go or feel it's a separate trip...”
- “A way of making the shop signs and displays have a more heritage feel to it would be good. A limit to the size of signage might also help...”
- “It seems a bit isolated. The Bon Accord Centre blocks the free flow of people to explore George Street. It almost as if the Bon Accord Centre is a final destination when coming from the south side with not much beyond it.”
- “I'd like to see the streets cleaner and a better quality of shops on George street.”
- “More recreational space, and green areas. It needs to be a more diverse area, so not just about retail.”
- “I think it would be an excellent street to encourage smaller independent shops to open rather than all the 'chain' shops. It could be made into a friendly welcoming street if some of the shop frontages were refurbished. At the moment it feels a bit run down which is a shame... There could be quite a variety of goods sold there as well as a baker and a grocer. All independent...”
- “Increase connectivity from adjoining streets for the students and pedestrians walking towards the college, Schoolhill and Union Street.”
- “I look forward to seeing a positive change in the area that further develops the community feeling and diversity that is already there.”

## SWOT Analysis

The following SWOT is a combination of influences from feedback from the consultation exercises and conclusions from spatial analysis. The SWOT analysis has helped shape the Masterplan detailed in the following sections.



### Strengths

1. Its location on the northern fringe of the City Centre, within a short walking distance of local amenities, the retail core and Marschial College
2. The NuArt festival includes Jopp's Lane and other areas of George Street, displaying an annual programme of murals, helping draw people into the area and add colour and vibrancy to the streetscape
3. Along with its listed buildings, there are buildings of character and interest that add a strong identity and townscape character to George Street
4. Strong links via bus to the immediate city centre as well as north to existing residential communities along George Street
5. Range of independent shops and businesses which play an important role in providing tertiary services for locals as well as the wider city. Includes some higher end and well regarded businesses including Finnies the family jewellers
6. A significant residential community who call George Street home who can support local services and amenities
7. Two of the city's major educational institutions in NESCOL and Robert Gordon's College, who between them attract a significant number of users to the area, and recognise the role of children and young people
8. Strong sense of local community and entrepreneurial, self starter spirit. Several successful organisations in and around the area including the George Street Community Council and Greater George Street Traders Association



### Weaknesses

1. Loch Street is wide and dominated by vehicles, creating separation between both Colleges and the core of George Street
2. Quality of the built form adjacent to NESCOL, industrial low storey units, is of a lower quality than the intact historic core and creates a visual disjoint
3. Arrival points to George Street area are poorly defined in some locations, namely on approach from Maberley Street, John Street, Spring Garden and Harriet Street. The experience along many of these connections for pedestrians is poor
4. The northern elevation of the Bon Accord Centre is a dominant, imposing and dead frontage. It reads as the rear of the building and parts are dominated by servicing areas
5. Harriet Street and Crooked Lane has poor quality urban form. Inactive façades, surface car parking and run down / vacant units to the rear create a poor quality experience and encourages anti-social behaviour
6. Limited wayfinding across the area and George Street is hidden from the rest of the city to the south (by the Bon Accord Centre) and from the west (by dominant highways infrastructure)
7. Quite a lot of street clutter and untidiness which weakens the aesthetic and townscape quality of the place; limited bin storage, over grown planting, seagull and dog fouling
8. Little to no basic necessities available such as accessible public toilets, information stands, water fountains or shading etc
9. Poor quality and distressed public realm materials and street furniture throughout the Masterplan area
10. Area has limited activity in the evenings and therefore can feel unsafe and uninviting. NESCOL evening classes are the exception which are offered throughout term time



### Opportunities

1. Form better defined routes in and out of the neighbourhood to avoid relying on pedestrian access via the Bon Accord Centre and Harriet Street
2. Reduce the impact of private car through journeys and create more space for pedestrians, cyclists and other modes of transport and active travel
3. Develop a cohesive approach to character of the public realm - through shop frontage, wayfinding and street materiality
4. Re-activate areas of low activity both short and longer term to address issues of vacancy early
5. Improved environmental maintenance and facilities across the area to tackle issues that impact on the day to day experiences of residents and businesses
6. Norco House invites opinion and interest, a recognisable piece of Aberdeen's social history which could be brought back to life as part of a mixed use development
7. Better promotion of George Street businesses
8. Creation of more dwell spaces across the neighbourhood
9. Introduce more urban greening to what is a hard environment through new projects
10. Further encourage start ups and independent retailers and businesses into the area and support those already there
11. Explore opportunities to encourage more residential accommodation into the area by offering a new form of residential offer
12. Engaging with young people through the Masterplan development process
13. Diversify the anchor of the neighbourhood, away from traditional retail to more mixed use, experiential and potentially cultural based offers
14. Develop stronger programme links to surrounding, well established institutions



### Threats

1. The George Street area remains visually hidden from the main pedestrian thoroughfares and areas of public realm investment planned across the wider City Centre and Beach Masterplan. Risk of the area becoming further detached (physically and psychologically) from the rest of the City if public realm is not addressed at the edges of the Masterplan area
2. The Bon Accord Centre and Norco House have recently changed ownership and the area could be at risk of further decline if no action is taken or the market fails to engage
3. Threat of demolition of Norco House if a suitable use isn't found and loss of embedded carbon
4. Threat of other areas in the city centre having a stronger and more inclusive presence than George Street and taking opportunity away
5. Threat of online and out of town shopping having a stronger appeal to some of the larger brand stores within the neighbourhood
6. Older and ageing populations being isolated due to poor service provision and public realm quality
7. Aging buildings can require more maintenance and may fall into disrepair if vacant for long periods of time
8. If the public realm falls into further disrepair, anti social behaviour will be encouraged in these areas and lower daily footfall

## Public Consultation April 2023

A draft version of the Masterplan was presented to the Council in December 2022 with a decision to undertake a 6-week public consultation in 2023.

This period of public consultation, which ran alongside ongoing stakeholder engagement, focused on the Draft Masterplan Vision, Strategy and proposed projects.

- 102 responses in total with broad support for the Strengths, Weaknesses, Opportunities and Threats (SWOT) analysis, Vision, projects, movement and regeneration areas
- Responses range from 50% to 80% support for the variety of proposals identified in the Masterplan

The table summarises the main feedback points from the Consultation under each of the questions asked.

This consultation was undertaken alongside wider engagement with stakeholders representatives and groups. More detail on the full consultation and their responses can be found in **Consultation Report**.

	Questions / Themes					
	Do you agree with the Vision	Do you agree with SWOT findings	Do you support making streets more attractive and creating greener spaces	Do you support changes to vehicular movement to improve place quality?	Do you agree with the list of projects and are any missing?	Is Norco House an important site for new uses?
% in agreement	<b>65%</b>	<b>61%</b>	<b>65%</b>	<b>65%</b>	<b>50% plus (24% somewhat agree)</b>	<b>80%</b>
Positive messages	- Pedestrianisation could really work - supporting accessibility and inclusivity - will bring people into the place - support cafés and outdoor seating	- Potential to create area for street food or socialising - possibility of becoming a true hidden gem by restricting traffic - spaces and activities that bring people together - Create new green space through demolition of Norco House - Giving more space to pedestrians and cyclists whilst removing spaces for parked cars	-Support community grow spaces - Support tree planting but needs to be done properly - Support cafe culture and increased lighting - Segregation of users is important - Existing businesses, schools and colleges need to be able to operate	-safety for all users important - existing businesses, schools and colleges need to be able to operate effectively - maintaining access for accessible parking	-creating sense of identity important to create reason to come - good to see outside seating and eating - more leisure uses and reducing crime may improve evening economy -more colourful play areas for children and activities e.g indoor golf - making more of existing heritage - considering younger generations and having independent businesses that meet their needs	-Preference for Norco House to be retained and re-purposed - ideas include Cultural & Community Mixed Use Hub Exhibition / Arcade Space Food Court Community maker space / pop up studios Multi-functional centre for cinema, museum, galleries, bars and food halls - an indoor Camden Market style location -creating a youth offer
% who disagree	<b>20%</b>	<b>16%</b>	<b>21%</b>	<b>21%</b>	<b>3%</b>	<b>11%</b>
Issues or concerns	- Vision is unclear - budget and funding concerns - Crime cannot be solved by street lighting alone - focus on young people - restricting access will make the area less inclusive	- too much focus on pedestrians - limit parking or create congestion will push people away - poor accessible facilities - no planning for student life -no green spaces for dog owners	- maintenance concerns - anti-social behaviour in new spaces -more consideration for elderly and disabled - budget concerns - concerns around traffic layout amends resulting in congestion elsewhere	- concern over proposed traffic layout - pushing congestion elsewhere - improve road links first then pedestrian routes	- lighting alone will not solve crime - more focus needed on car access and parking or people will not visit - maintenance budget - delivery budget. Start with easy improvements / quick wins	- Concerns of residential use -Concerns over oversupply of space in Norco House when there are already vacant shops -Ongoing maintenance of the building

**Table 1 - Summary of April 2023 public consultation responses**

## Potential Projects Suggested Through Consultation Responses

Both the public consultation exercises in February 2022 and April 2023 have identified a number of potential projects that the people of George Street would like to see within the Masterplan area.

These were analysed and evaluated to help inform the development of the Masterplan. Those interventions identified by the public and stakeholders that could potentially be identified under the remit of the Masterplan (both by public and private sector) are set out below;

### Potential place projects

- **Enhance and modernise the streetscape** - this could include enhancements to the public realm, de-cluttering and provide improved pavements, places to dwell and spaces to host street events
- **Addressing vacant properties** - helping to reactivate the area and reduce perceptions of decline / dereliction
- **Introduce space / facilities to help support temporary and meanwhile uses** - helping to bring more activity and animation to the area. This type of intervention could include pop up events, temporary road closures, temporary seating, planting features, signage and wayfinding. The space created would also allow businesses to expand out into the street to host events
- **Improving the pedestrian experience** - this could include enhanced materials, low level lighting, wayfinding, pedestrian prioritisation measures, better managed and potentially reduced street parking as well as planned cycle lanes and safe cycle storage to encourage more cyclists into the area
- **Improve safety and lower crime rates in the area** - such as more CCTV, better street lighting and greater levels of activity (through different uses and types of spaces) to improve the sense of safety both during day and evening
- **Introduce more public space & public facilities through demolition** - a more radical suggestion raised was to demolish both the Bon Accord Centre and Norco House and to replace with public facilities, public space, green space
- **Norco House** - retaining the current building and converting it to a mixed use leisure and cultural destination for the area and wider city. Elements of community uses could be included as well

### Potential people and economy projects

- **Creation of a brand identity** - to promote and recognise the area and promote visits to George Street. A place and online based campaign that promotes the Masterplan area to a wider audience
- **Expanding events** - working with the council, local community groups and stakeholders to expand the city centre events programme to include the George Street area
- **Help to facilitate local communities** - feedback suggests the existing community is strong and well connected, however a coordination group could be set up to help local residents focus energy and effort
- **Ongoing inclusion and collaboration** - particularly with local residents and stakeholders throughout the design and decision making process
- **Attracting start-ups** - creating the right environment to encourage local entrepreneurs to set up in the area

Precedent image: Playful interventions on a quiet street and a dead frontage bring animation to the street



Credit: Smith and Lewarne

## **2. George Street - The Place**

# 2. George Street - The Place

**Understanding the characteristics of a place is part of the process in developing the Masterplan. This understanding of 'place' and its features allows opportunities for change and enhancement to be explored.**

The most pertinent influences affecting the Masterplan area are:

- 1. Areas of influence** - understanding the surrounding areas where people work, live or visit in large numbers who interact with and which could bring footfall to the neighbourhood;
- 2. Activity and vibrancy** - understanding where areas are busy and active and where others suffer from inactivity, and consequently issues around anti-social behaviour and decline;
- 3. Local character** - a review of the local buildings, materials and features to understand which areas are most characterful;
- 4. Land uses** - a review of existing land-uses within the neighbourhood and understanding the mix and location of uses;
- 5. Legibility** - understanding how easy an area is to get to and navigate around;
- 6. Access and movement** - understanding the movement and flow of pedestrians, cyclists, buses and cars across the area;
- 7. Public realm quality** - a review of the quality of the public realm and its capacity to support change
- 8. Existing green spaces** - understanding what provision of public space exists in and around the area and how easy it is to get to them
- 9. Typical street sections** - a review of the existing physical features of each of the streets within the Masterplan area.

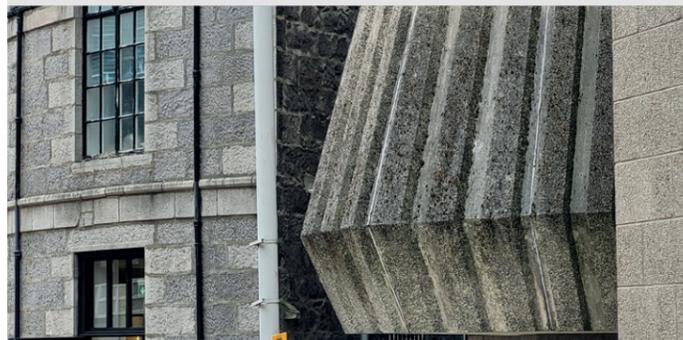
Additional place analysis that has informed the Masterplan can be found in **Appendix 2 and 3.**



An area of influence - Marischal College & Square



Vibrancy - NuArt Festival street art adding colour to otherwise blank walls



Distinct local character - variety of architecture and materials creating a character to George Street



Public realm quality - varying quality and materials along a key pedestrianised part of George Street

## 1 Areas Of Influence

George Street sits to the northern fringes of a dynamic part of Aberdeen City Centre.

To the south lies a number of leisure, retail and cultural destinations and established city quarters. It sits next to the retail core of the City and is a short distance to Union Street and the recently refurbished Union Terrace Gardens. The cultural and leisure areas of Belmont Quarter, Merchants Quarter and Ship Row are within a short walk and some of which are undergoing phased regeneration programmes.

Immediately adjacent to the study area are a number of significant civic and education institutions which form important destinations within the city, generating large numbers of footfall and which could further influence the future role of the Masterplan.

Importantly, the study area is also surrounded by existing residential neighbourhoods to the north, with several existing residential communities located northwards, along and near to the George Street corridor towards Kittybrewster.

The future role of the George Street study area must look to respond to these surrounding influences and to help support emerging regeneration priorities whilst supporting those existing residential, civic and educational communities that currently exist.

There are 7 immediate areas of influence which will directly influence and shape the future character, identity and role of George Street. These are:

**Broadford Works** - a proposed new residential and mixed use community to the north of the study area (estimated around 890 new homes). When delivered, Broadford Works will influence George Street with its future residents making use of its local amenities and pedestrian/ cycle through journeys to the City Centre.

**North East Scotland College (NESCOL)** - a significant stakeholder within the Masterplan boundary. It is home to a student population studying and living within the area. The Masterplan should look to support the needs of the students and visitors to the College and ensure it facilitates improved connections and access to the College. Students will also likely pass through the study area to access outdoor, retail and leisure destinations to the south and therefore the role of streets, activity and spaces will be important.

**Robert Gordon's College & Art Gallery** - one of Aberdeen's most prestigious education institutions and the Art Gallery being an important cultural asset. Both have an indirect physical connection through to George Street which could be improved. The large number of daily visitors presents an opportunity for George Street to improve links and attract more visits and trips into the area by foot and bike.

Both NESCOL and Robert Gordon's College have the potential to see a significant amount of students, staff and parents use their facilities both in the daytime and evening. The Masterplan should seek to support the functions of the college as well as capturing the potential benefits of such a significant number of people attending both institutions.

**Marischal College** - an important civic and cultural destination within the city, located immediately east of the study area. It presents a significant influence both physically (townscape quality and distinctiveness) as well as creating potential footfall, as the location of Aberdeen City Council and other Public Sector bodies. Improved legibility and links into George Street to this area should be explored.

**George Street** - the historic corridor characterised by traditional granite buildings and active ground floor uses is itself a key influence on the rest of the study area. The main onward connection to Union Street is via a private connection through the Bon Accord Centre. The extent of influence of George Street is currently contained to its corridor but in the future it could expand activity both within the neighbourhood and towards the City Centre.

**Bon Accord Centre** - a significant influence and is one of the primary shopping destinations within the city centre. The Bon Accord Centre is located along the historic alignment of George Street, stopping up the direct connection to Union Street and now forms a private connection through to Upperkirkgate. The relationship of the building with George Street in terms of impact on pedestrian movement, activity and visual impact have potential to improve the overall experience of the neighbourhood.

**Norco House** - Northern Co-operative Society's Norco House (most recently occupied by John Lewis) was built in 1966-70 for the (now-defunct) Northern Co-operative Society. The building is an example of brutalist, modernist architecture and intended to be a showpiece within the city's architectural make-up. During the Covid-19 pandemic it operated as a temporary NHS vaccination centre. The building is a distinctive local landmark and has significant potential both as a retained building and redevelopment site to transform the southern end of George Street.

-  George Street Masterplan boundary
  -  Infrastructure restricting east-west pedestrian movement
  -  Local destinations driving footfall
  -  Union Street Terrace Gardens
- Key Areas of Influence:**
-  **Broadford Works** - a proposed residential-led mixed use site within walking distance to George Street & Upperkirkgate (via Bon Accord Centre)
  -  **North East Scotland College (NESCOL)** - significant student population within the Masterplan
  -  **Robert Gordon's College** - educational institution with potential to better connect to George Street
  -  **Marischal College** - important civic location with potential to encourage more visits to George Street
  -  **George Street** - the spine of the neighbourhood that influences the scale and level of vibrancy and activity across the Masterplan
  -  **Bon Accord Centre** - a significant influence on the -masterplan with regard to visual, experience and movement into and out of the Masterplan area.
  -  **Norco House** - distinctive, brutalist building which has potential to be a key destination point within George Street through re-purposing or redevelopment.



**Figure 2 - Areas of Influence within and surrounding George Street**

## 2 Activity And Vibrancy

Activity and vibrancy are important features of a safe and welcoming urban environment. The levels of activity along streets can help promote passive surveillance, which in turn can help make places feel safer and more inviting, encouraging more people to visit and dwell. The level of activity and vibrancy will also depend upon the types of uses and their design relationship with the street. Some intentionally promote activity, whilst others create dead frontages which can cause a lack of passive surveillance and sense of safety.

The plan adjacent illustrates areas where frontages with high and low activity are located. It illustrates where certain areas experience greater levels of activity / or have higher levels of 'eyes on street' making them feel safer and more inviting. Conversely it also highlights areas where buildings have few / no window or door openings creating unanimated and dead frontages.

George Street and the immediate junctions with John Street and St. Andrew Street are where the greatest amount of ground floor activity is focused, facilitating the highest concentration of street activity and a sense of passive surveillance.

Craigie Street and Charlotte Street experience some degree of passive surveillance given their residential character. Front doors and property windows overlook the street and create some degree of passive surveillance.

In contrast, streets such as Spring Garden, Loch Street and the southern end of Charlotte Street have lesser amount of active frontages and building windows / doors at street level. This creates a noticeable reduction in building activity and sense of 'coming & going'. Buildings being predominantly residential, built as enclosed, inward facing apartment blocks, with only a few communal entrances.

Towards the south of George Street, particularly along Loch Street, Harriet Street and around Norco House, building frontage activity and passive surveillance is extremely low or absent due to vacant properties and dead frontages.

### What this means:

- Streets with low or no active frontages negatively impact upon perceptions of 'place', often making them feel unsafe and unwelcoming. This discourages visitors or encourages people to use alternative routes through;
- Future proposals should respond to the levels of activity along each street. For example providing more space for activity along George Street to maintain and increase activity.
- Future interventions should also address areas of low activity by introducing improvements to further increase levels of activity to reduce anti-social behaviour and poor legibility.



Active ground floor uses creates 'eyes on street'



Dead frontages and no passive surveillance

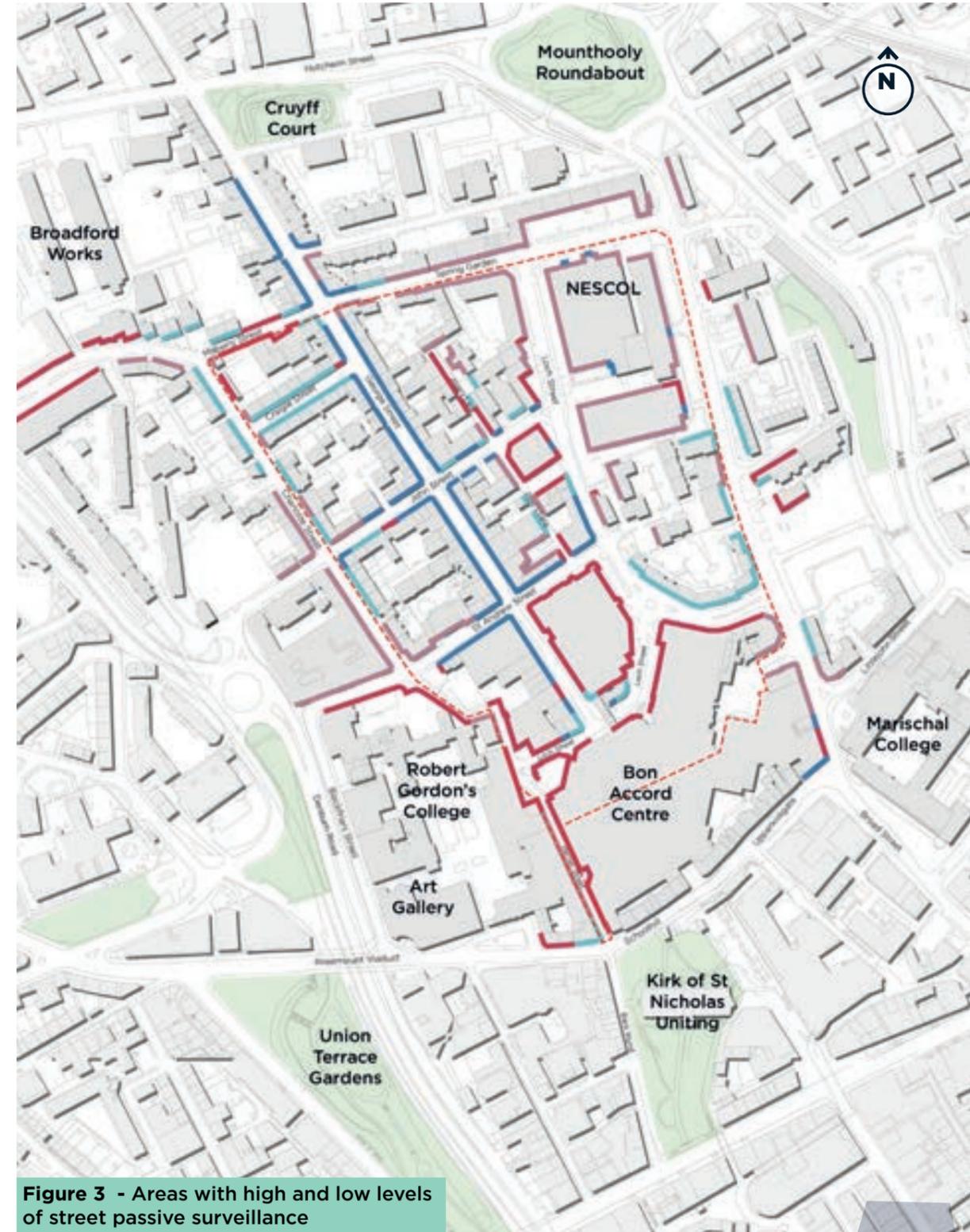
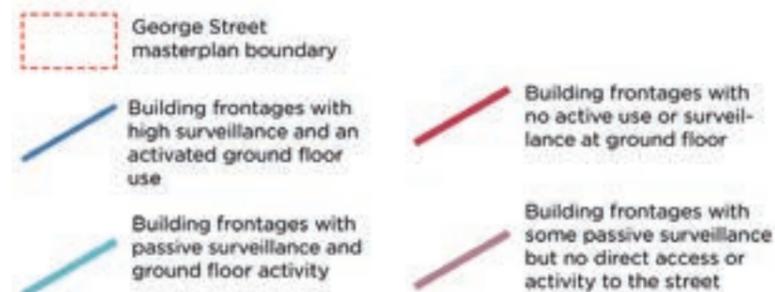


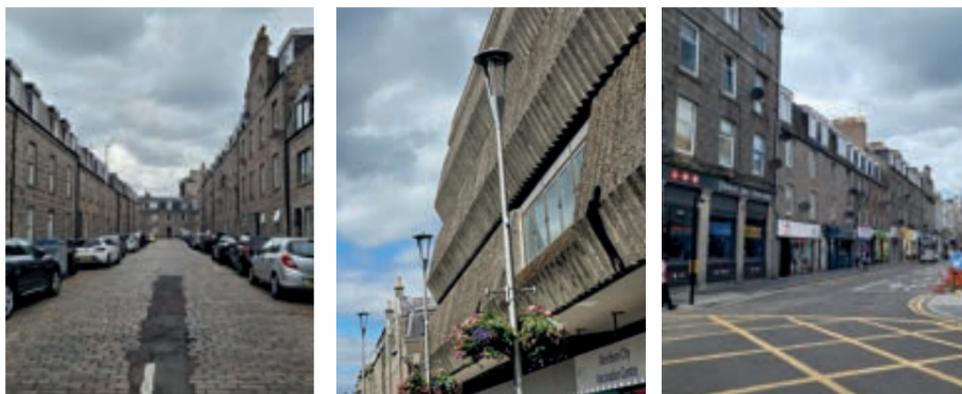
Figure 3 - Areas with high and low levels of street passive surveillance

### 3 Local Character

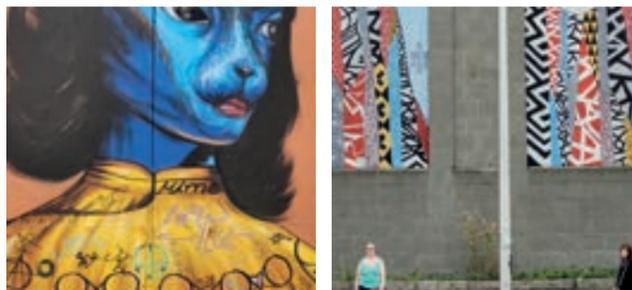
The Masterplan area has a varied mix of built form character which combine to create an overall impression of place and identity. The plan adjacent broadly identifies five broad areas that can be grouped or characterised based upon their built form characteristics.

-  Victorian or older grey granite buildings with distinct facade and roofing detail and expressions that create a pleasant and attractive townscape;
-  Areas with predominantly grey granite / rendered façades and which have a lesser degree of architectural detailing and character
-  Brutalist / geometric larger massing forms
-  Former industrial sites add character through their intact chimneys and built form scale
-  Buildings with a lack of facade detail or expression creating poor quality edges to the street

In addition to the built form of the area, there are also pockets of colour and vibrancy such as spray painted wall murals as part of the NuArt festival. These bring colour and interest that contrasts to the granite grey consistency of the area.



Variety of positive and negative character of George Street



Burst of colour as part of NuArt mural festival adds contrast to the Masterplan area and could be further expanded.



**What this means:**

- George Street's character should be reinforced through public realm improvements
- Improvements to areas with intact historic features should look to enhance and celebrate
- Areas of weak character should be focused on using a variety of interventions to reduce their negative impact.
- Street art successfully animates & adds colour contrast in areas with poorer quality townscape which should be further encouraged.

## 4 Land Uses

George Street within the Masterplan area is predominately a mixed-use urban high street. It is comprised of primarily granite tenement buildings with small local retail shops at ground floor, complemented by residential units on the upper floors.

This retail offer is dominated by a variety of independent health and beauty shops, hairdressers, takeaways, cafes and repair shops, creating a vitality to the area.

Despite its bustling communities, there are a number of vacant units to the south of the George Street corridor. There are also vacant units along George Street and within the Bon Accord Centre, however, some of the vacant units are being used for interim uses and/or being brought to market, presenting an evolving context of change.

At the time of writing, the Norco House site which had been predominantly vacant since the closure of John Lewis, has been purchased by the new owners of the Bon Accord Centre.

### Residential

George Street plays an important role within the wider city centre, offering people the opportunity to access more affordable residential accommodation within the city centre. It provides a sustainable and well connected urban neighbourhood to allow those who choose to live there access to Aberdeen's city centre amenities.

The area has struggled in recent years to secure new residential growth, partly linked to lack of development sites within the Masterplan area, however there are residential development sites identified immediately beyond which could increase the choice of residential accommodation around George Street.

### Retail

The retail market on George Street has historically been split in two, with more national retail sitting south of St. Andrew Street, and local business sitting to the north. The presence of the former John Lewis adjacent to the Bon Accord Centre attracted national retailers such as Santander, Blacks, Magnet Kitchens and Greggs occupying larger units at the south end of the street. These created and benefited from increased footfall brought by the department store.

George Street also has a well regarded local business in Finnies, the Jewellers, which attracts city centre visitors and those further afield along George Street.

However, lower George Street's retail market dynamic has been impacted by the closure of John Lewis. The Bon Accord Centre also has a current planning application for expansion however at the time of writing the Bon Accord Centre has recently changed ownership.

In contrast, the retail market to the north of George Street comprises smaller, local and independent retail units offering lower value everyday items to the market, serving the people that live there. These include local takeaways, beauticians and cafés.

### Education

The presence of NESCOL within the study area and Robert Gordon's College to the south west are important local institutions that interact with the Masterplan area. Their student populations frequently travel to their respective colleges and this presents an opportunity to capture daily trips. Both college's curriculum will drive and influence the future uses within George Street and the Masterplan should look to ensure the ability to support and respond.

### Existing and Emerging Business

Across the George Street neighbourhood, there are several organisations such as George Street Community Council and the Greater George Street Traders Association, that are operating in the area with each having their own distinctive role. However, it is apparent through local studies that each lacks a knowledge of what the other is doing and how they could support each-other. There lies the possibility to create a Community Network where groups, organisations and individuals would be asked to share ideas and take forward some actions that had come from the event and further consultations that exist for the area.

#### What this means:

- Improvements to the public realm and streetscape could help local businesses to expand and promote themselves
- Identify and tackle voids or vacant units to reduce the impact on negative perceptions of the place
- Proposals should help education institutions better physically connect with the area as well as consider localised projects to address issues around anti-social behaviour and improving the experience to stay for longer.
- A coordinated approach by businesses and residents in the area will help create an identity for George Street that can be promoted online and on the ground.



Mix of vacant and trading ground floor uses creates disjointed levels of activity

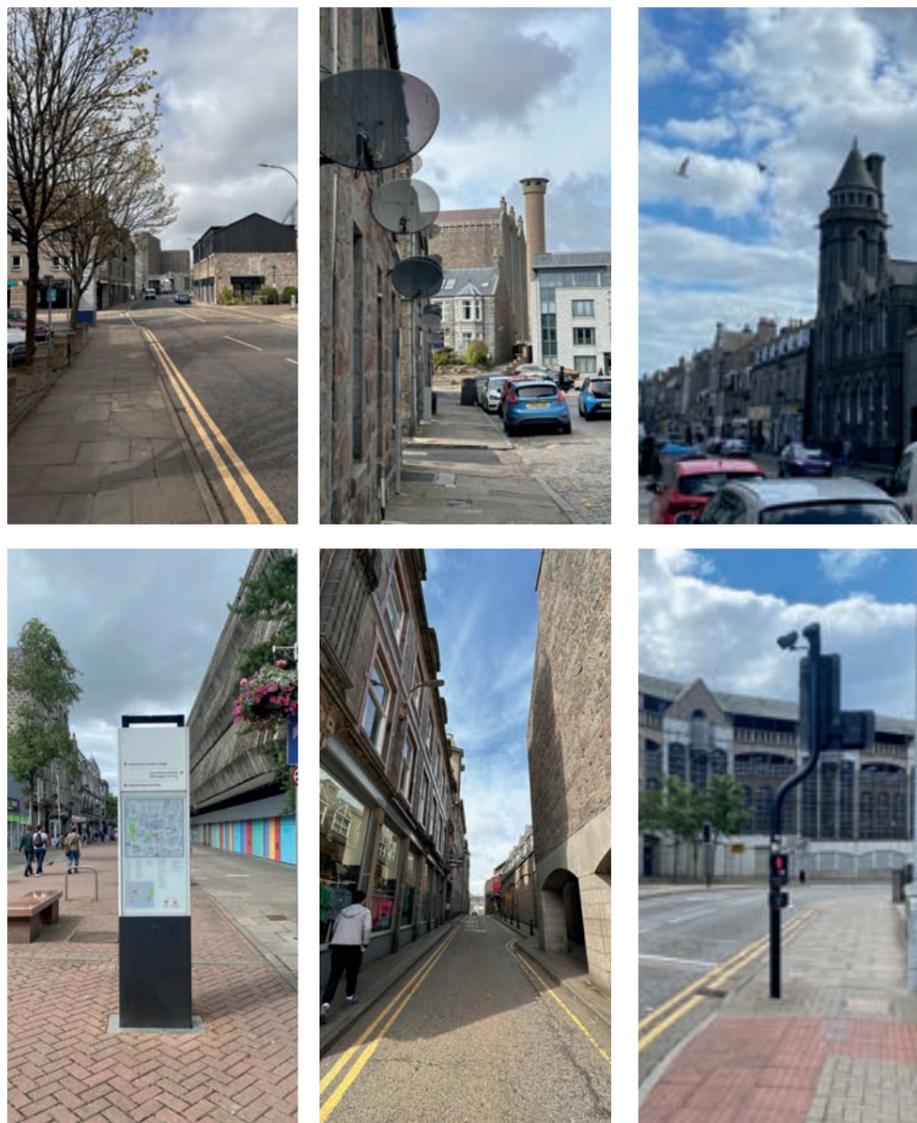


Existing signage on George Street

## 5 Legibility

The historic grid layout in the heart of George Street is intact, with many of its streets exhibiting a number of features and characteristics which provide design cues and influences for people as they move through. However, the edges of the study area are impacted and interrupted by larger, vehicle dominated roads and infrastructure. Some of these features help to give a sense of place and character;

- The historic grid helps orientation north - south along George Street as well as east-west
- Long range views and vistas along grid of streets within the study area towards larger imposing buildings / structures beyond help legibility within the study area
- Legibility breaks down further east towards A96 and along Loch Street and Harriet Street
- A number of attractive & prominent architectural features on street corners help reinforce local points of activity / focus
- Larger vehicle dominated points of arrival e.g Mounthooly roundabout create compromised pedestrian desire lines in and out of the Masterplan area, and
- Internal courtyards of residential courts prevent pedestrian permeability and through movement.



Architectural features create interesting long range views whilst the wayfinding experience at street level is often mixed

### What this means:

- The approaches to the George Street area Masterplan for pedestrians are vehicle dominated and unclear
- This means clear and well co-ordinated signage, wayfinding and altering the current street layout may be required to help people to easily understand where to go and to ensure that they can do so comfortably.

- ↗ Views towards areas of interest / building features of interest
- Buildings with features of interest
- Places that have a clear sense of centre
- ↖ Long range views north of George Street
- ▬ Entrance streets to George Street with poor legibility, wayfinding and vehicle dominance



Figure 5 - Legibility

## 6 Access and Movement

### City Centre Movement

Aberdeen City Centre is accessible using a variety of modes and routes. The rail station provides access for local towns to the northwest and south of the city, for commuter and retail purposes, as well as more strategic access to Dyce Airport and longer-distance trips to Inverness, Edinburgh and beyond. The city centre is well served by a network of bus routes, providing radial access to and from adjacent towns and residential areas, as well as key land-uses including Robert Gordon and Aberdeen Universities, hospitals including Aberdeen Royal Infirmary and Aberdeen Harbours.

### George Street

The George Street Masterplan area is a walkable neighbourhood with a number of pedestrian routes linking to surrounding neighbourhoods and city centre. It is also well connected by bus to the wider city centre.

George Street is a multi-functional street with pedestrianised access to the Bon Accord Centre to the south and all vehicle movements in each direction to the north of St. Andrew Street. It serves local bus routes, has pedestrian activity on both sides and also contains some advisory cycle priority measures along its length. In addition, the combination of on-street resident permit and short-stay pay and display parking serving the mixed residential and commercial frontages along George Street ensure that the competition for the available streetspace is significant.

### Pedestrian And Cycle Accessibility

The cycle network is limited to the designated National Cycle Network Route 1 which runs along Gallowgate, on the edge of the Masterplan area connecting Loch Street to Upperkirkgate and across to Union Terrace Gardens.

There are 3 main pedestrian choices for access southwards into the City Centre. There is limited direct access to Schoolhill / Upperkirkgate, which is a private connection through the Bon Accord Centre.

Harriet Street provides access for pedestrians but the quality of the experience and the provision of footway space is poor due to the narrow street and dead frontages along it.

Loch Street / Berry Street also provide pedestrian access towards Marischal College but again the quality of the experience could be improved due to dead frontages along the Bon Accord Centre and streets designed to accommodate vehicles.

### Public Transport

St. Andrew Street, John Street, George Street, Loch Street and Gallowgate are streets which service the local bus routes in the area. These are important for access and connections to Aberdeen's Bus Station which acts as a travel hub for the city centre, as well as connections to further afield, beyond the city centre. John Street is one way in the eastbound direction from Charlotte street to Loch Street, while St. Andrew Street is not entirely one way westbound, with general traffic able to travel eastbound to the junction with George Street.

### Vehicle Movement Function

Streets within the study area have a wide range of functions, varying from those with greater number of vehicular traffic volumes to those that are much quieter and function as local access streets. The plan adjacent highlights the significance and function of streets within the study area; Primary, Secondary and Tertiary routes. Arrows also denote which of these routes are one way.

#### What this means:

- Some streets have the capacity to be reconfigured to allow for more pedestrian and public realm space
- There are safety concerns around pedestrian/cyclist conflicts with cars, and local air quality
- The CC & BMP Vision for access and movement is to make the city centre a **place to go to and not through.** George Street Masterplan will identify projects that support this wider objective
- Future projects to will promote pedestrianised zones and vehicle restricted areas



Figure 6 - Local Access (All Modes)

## 7 Public Realm Quality

The quality of public realm within George Street is varied and is predominantly medium or low quality across the Masterplan area.

There are some examples of recent improvements to the quality of public realm materials (for instance, along the northern side of George Street), but these tend to be isolated and sit as part of low-medium quality streets.

### Ongoing improvements across the wider area

Beyond the Masterplan area, there are a number of locations which have undergone recent public realm improvements. These spaces have a higher quality of materials and signage and which provide an interface to some streets within the Masterplan area - for example the improved public realm along Back Wynd contrasts to that of Harriet Street immediately opposite. At present, because of the lower quality of materials, George Street feels cut off and disjointed. There is an opportunity to further expand upon recent city-wide improvements into George Street to create more seamless journeys and experiences into and out of the Masterplan area and to create vehicle hierarchy within George Street.



#### What this means:

- Enhancing the quality of materials in the public realm will create inviting and safe environments that are accessible by all users and where people are encouraged to visit and dwell.
- Additional seating elements are required across the George Street area to improve accessibility and to create areas for dwell space.
- Bin storage is a problem across the entire area and negatively impacts visual quality of the streetscape.

### Medium quality materials

Elements of high quality spaces including street furniture and trees, but with a dominance of lower-quality materials and/or damaged materials.

- Vehicles are prioritised within the streetscape.
- The public realm is accessible to all users and feels safe at certain points.

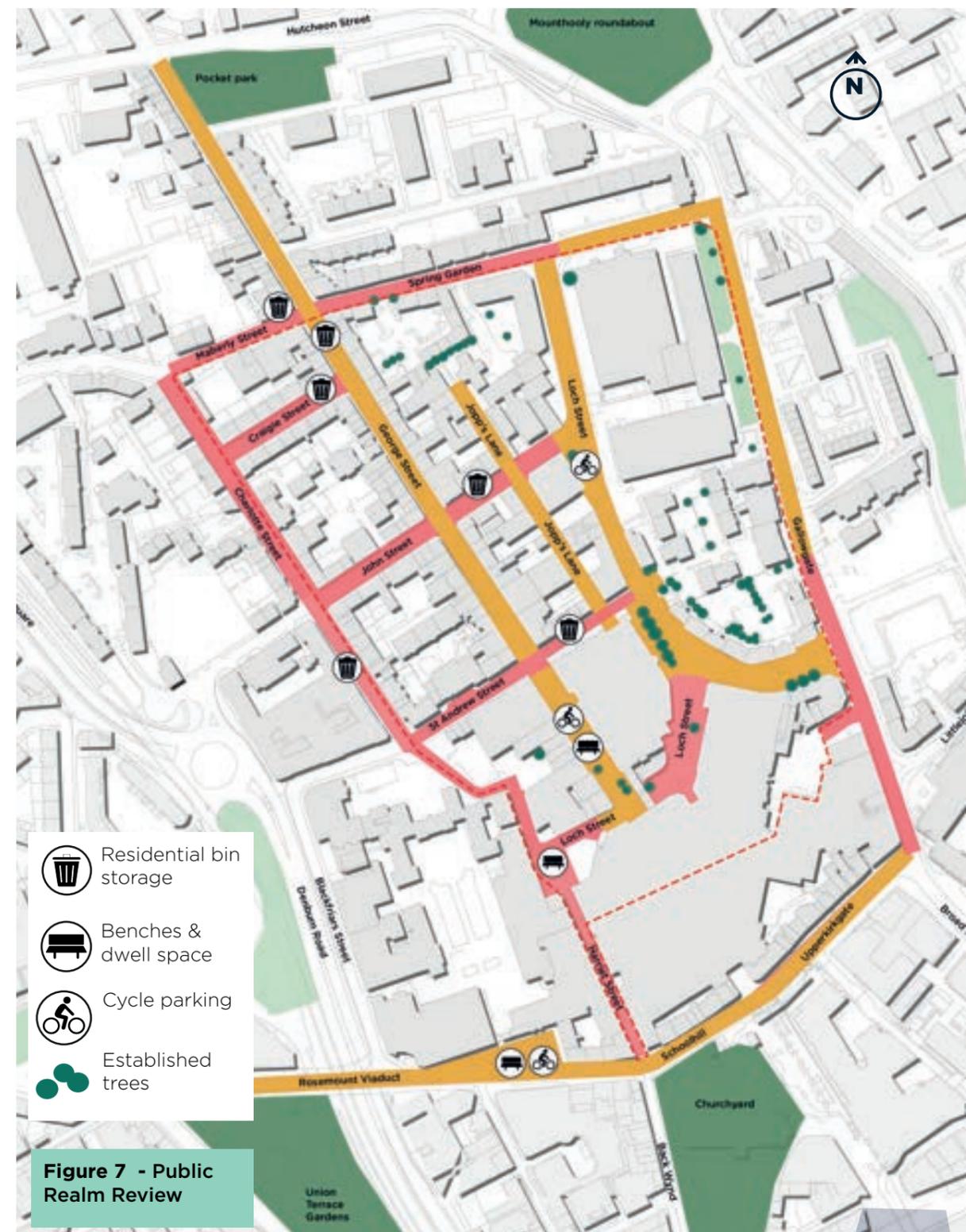
### Low quality materials

The streetscape is lacking in both high quality materials and street furniture.

- Vehicles are prioritised within the streetscape.
- The public realm is not accessible to all users and does not feel safe.

### Damaged and dated

There are a number of public realm materials and street furniture that are damaged, or in decline that would benefit from being replaced. There are also areas of outdated street furniture and each with a different aesthetic. All of these issues add to an overall negative impression of place and disjointed appearance.



**Figure 7 - Public Realm Review**

## 8 Green Space

There is a lack of accessible and functional green space within the George Street area. Existing street trees within the public realm are either struggling, or have died.

There is an opportunity to incorporate functional green space to soften the surrounding hardscape and perform a number of valuable functions. These might include well-being benefits, nature-interactions, and stormwater management.

There are existing areas of greenspace within walking distance of the study area however the quality of the pedestrian journey to and from these spaces is often uncomfortable, due to conflict with vehicles or narrow pedestrian footways or more difficult to navigate. A summary of these issues is set out below;

- **Route from Union Terrace Gardens:** Cuts through Harriet Street which feels unsafe due to the lack of surveillance and pedestrian blind spots.
- **Route from Churchyard:** Most straightforward route involves cutting through the shopping centre demonstrating lack of connectivity to George Street.
- **Route from Pocket park / Cruyff Court:** Main route is along vehicular dominated streets.



### Accessible green space

Accessible green spaces are currently located outside of the George Street area. Notable nearby green spaces include Cruyff Court set within a small pocket park, Union Terrace Gardens and the churchyard on Schoolhill. There is a need for more accessible public green spaces within the George Street area, as these currently fall outside of the Masterplan area.

### Non-functional green space

Whilst there are 'green' spaces within the George Street area, these tend to exist as grass verges adjacent to roads, or are set behind a boundary adjacent to residential property. There is a clear need to enhance existing green spaces so that they perform an environmental, social, and aesthetic function.

### Established trees

Established trees in George Street are mostly located along main vehicular roads or within residential courtyards. There is an opportunity to incorporate tree planting within the public realm to perform a number of social and environmental functions.

#### What this means:

- Although there are accessible green spaces within a 5 minute walk of George Street, the routes to these spaces are of a low quality for pedestrian experience and safety.
- Existing accessible green spaces are also limited in the types of functions they perform and users they attract. Additional green spaces which serve a wider range of users are required.



Figure 8 - Proximity to existing green spaces

- Established trees
- Non functional green space
- Accessible green space
- Approximately 500m radius from green space (5 minute walking distance).
- Pedestrian route from green space to George Street.

## 9 Typical Street Sections

George Street's street grid arrangement of streets has its own character. They are home to a variety of independent businesses, national retailers and residential tenements.

Streets are typically dominated by vehicles meaning road widths are wide and pedestrian footways and cycle provision are compromised. This encourages more journeys by car than by foot or bike. The majority of streets within the Masterplan area are composed of historic buildings on both sides, creating an often narrow street section which also adds pressure to street function.

The following section details the existing street profiles for each street within the Masterplan area. By understanding their physical features we are able to understand the potential to reconfigure to meet the aspirations of the Masterplan. The sections shown are a typical street section and more detail will follow in the transformational projects section.



The image above defines the extents of the street, carriageway and road.

Street Design: includes everything captured between two facing building fronts.

Carriageway: includes the road, on-street parking, tree planting, bike storage, cycle lane where relevant, bin storage and planting strips and the footways concern areas used only for pedestrian movement.

Road: only the width of the space for moving vehicles.

Typical Existing Street Characteristics	Street Name		
	A - Charlotte Street	B - Craigie Street	C - Spring Garden
Street Design			
Carriageway width	8.88m	9.36m	7.32m
Road width	3.88m	4.96m	5.12m
Footpath width	3.75m / 2.22m	1.88m / 1.79m	1.53m / 2.37m
On Street parking?	Y	Y	Y
Loading bay requirement on-street?	N	N	N
Bus route and stops	N	N	N
Materiality			
Carriageway	Granite setts	Granite setts	Hot rolled asphalt
Footway	Slabs	Paving slabs	Hot rolled asphalt
Streetscape			
Predominant fronting building use	Residential	Residential	Residential
Refuse storage on street?	Y	Y	N
Street planting?	N	N	Limited to boundary planting within property boundary
Street furniture?	N	N	N

Table 2 - Existing Street Design

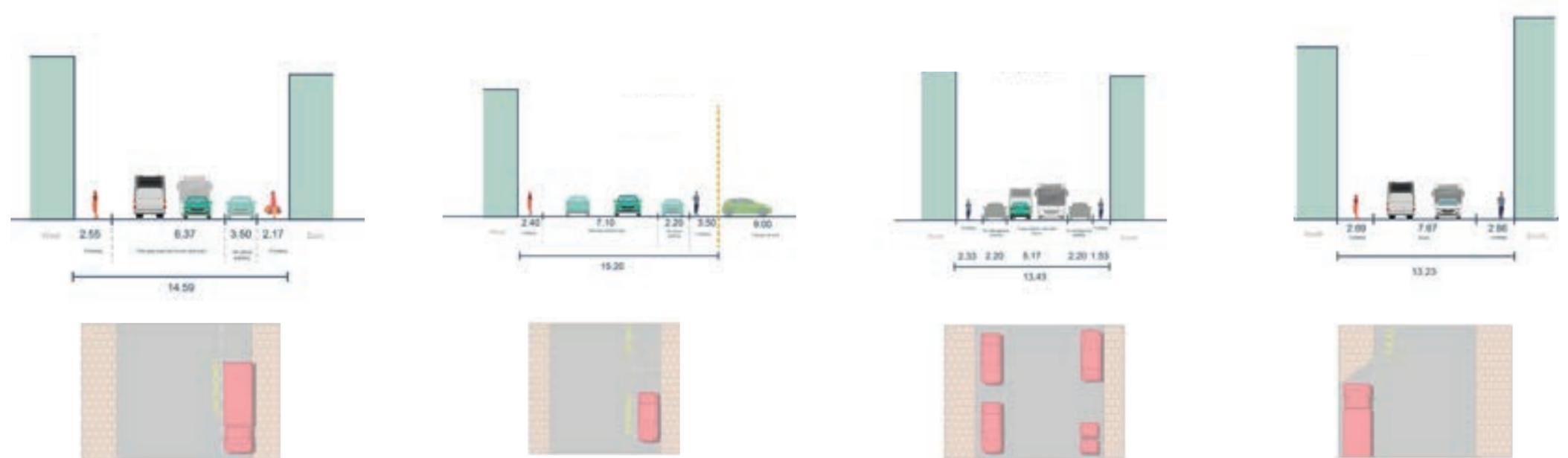


Figure 9 - Section Reference



Typical Existing Street Characteristics	Street Name			
	D - George Street	E - Loch Street	F - John Street	G - St. Andrew Street
Street Design				
Carriageway width	9.86m	9.30m	9.57m	7.67m
Road width	5.46m	7.10m	5.17m	5.48m
Footpath width	2.20m / 2.17m	1.60m / 1.95m	2.33m / 1.53m	2.69m / 2.86m
On Street parking?	Y	Y	Y	N
Loading bay requirement on-street?	Y	N	Y	Y
Bus route and stops	Y	N	Y	Y
Materiality				
Carriageway	Hot rolled asphalt / setts	Hot rolled asphalt	Hot rolled asphalt	Hot rolled asphalt
Footway	Hot rolled asphalt / setts	Hot rolled asphalt	Hot rolled asphalt	Hot rolled asphalt
Streetscape				
Predominant fronting building use	Ground floor commercial / retail / residential front doors	Commercial / education / residential	Commercial / retail	Commercial / retail
Refuse storage on street?	Y	N	Y	Y
Street planting?	Y	Y	N	N
Street furniture?	Y	N	N	N

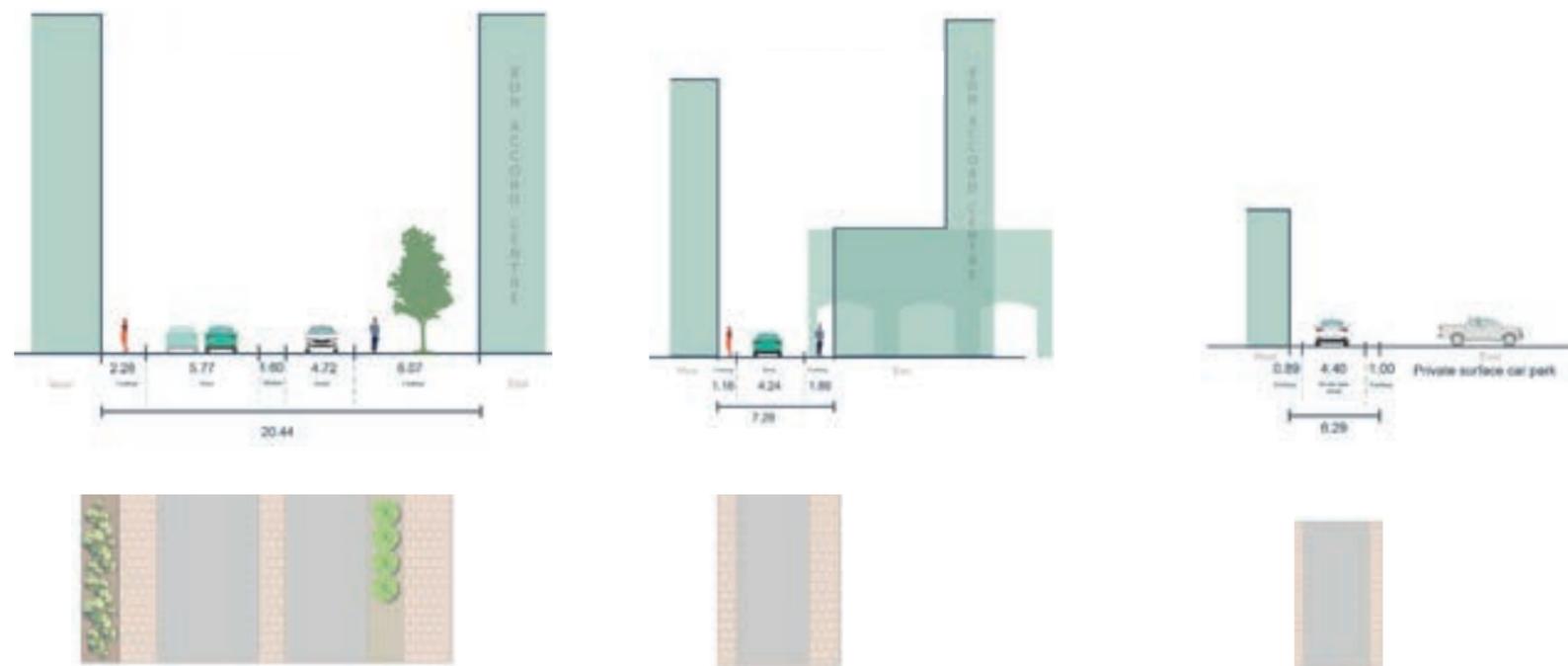
**Table 2 (contd) - Existing Street Design**



**Figure 10 - Section Reference**

Typical Existing Street Characteristics	Street Name		
	H - Berry Street	I - Harriet Street	J - Jopp's Lane
Street Design			
Carriageway width	11.96m	4.24m	4.40m
Road width	5.75m/4.73m	4.24m	4.40m
Footpath width	2.16m / 4.75m	1.16m / 1.66m	1m / 0.89m
On Street parking?	N	N	N
Loading bay requirement on-street?	N	N	N
Bus route and stops	Y	N	N
Materiality			
Carriageway	Hot rolled asphalt	Hot rolled asphalt	Hot rolled asphalt
Footway	Paving slabs	Hot rolled asphalt	Hot rolled asphalt
Streetscape			
Predominant fronting building use	Commercial	Commercial	Commercial
Refuse storage on street?	N	N	Y
Street planting?	Y	N	N
Street furniture?	N	N	N

**Table 2 (contd) - Existing Street Design**



**Figure 11 - Section Reference**

## **3. The Masterplan**

# 3. The Masterplan

The Masterplan is informed by a clear Vision and objectives, which respond to the stakeholder feedback and engagement.

It identifies a series of transformational projects that will help to deliver the Vision and our understanding of the place.

## Vision & Objectives

George Street is supported to be a place to form new connections; Social, Cultural, Economic and Educational. An authentic well-connected neighbourhood with a strong sense of communities and a better sense of place, enabling people to come together and enjoy their neighbourhood.

The Masterplan seeks to capture the potential of the area's physical characteristics, its diverse community and the future needs of the area to allow it to thrive. We will do this by focusing on 'place' and supporting 'people' and 'economy':

- **Embracing the difference** - highlighting the quirks & celebrate the variety of George Street
- **Building its reputation** - encouraging people to discover the area
- **Creating new reasons to come to the area** through transformational projects and addressing areas of decline
- **Unlocking big changes through incremental interventions.** Using meanwhile uses / pop ups and short term/trial interventions to experiment with ideas
- **Fixing the edges** and enhancing the arrival
- **Demanding more from the streets** - inviting more pedestrian activity but managing vehicular movement
- **Creating a greener form of urbanism** & a more liveable environment.



A visual representation of the Masterplan Vision, capturing some of the elements that can come together to help deliver the Vision's ambitions



**Vision:**

**George Street Connected!**

**The 'local' Neighbourhood: a vibrant, creative and inclusive city centre community at the heart of reinvention.**

## Masterplan objectives

### 1 George Street as a destination

A hidden gem within the City with the potential to further define its cultural offer and identity. The Masterplan will support interventions that add to the burgeoning scene for arts, crafts and independents across the City as well as establishing Norco House as a potential future mixed use destination. The area can continue to be a distinctive and greater cultural part of the city.

### 2 Establishing a distinctive character for George Street

Creating a colourful street aesthetic with a co-ordinated palette of quality materials, planting, wayfinding and street furniture distinctive to George Street.

### 3 A sustainable city centre neighbourhood

Creating an enhanced, greener, nature rich street-scape environment, through SUDS features, new landscape & planted spaces, greening & pollinator friendly features and new tree planting as well as promoting safe, sustainable movement. Improving infrastructure to support sustainable city centre residential living.

### 4 Adapting streets

Making the streets within the Masterplan work harder, reclaiming spaces, where possible, to prioritise people, creating more spill out space for local businesses, create more dwell space for residents and manage traffic movements through the neighbourhood.

### 5 Finding space

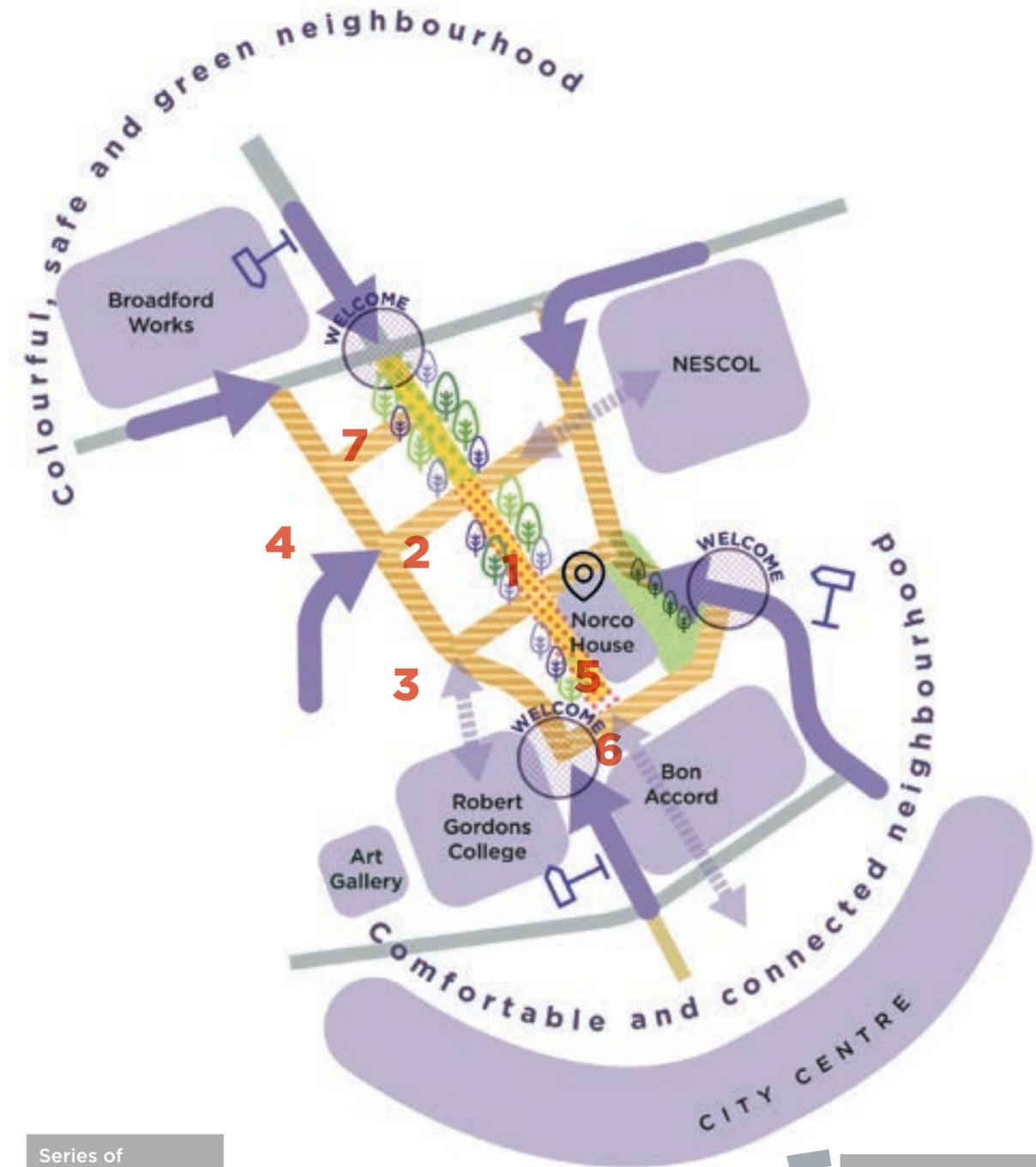
Creating a new neighbourhood space that encourages community and individual well-being, opportunities for social interaction and relaxation.

### 6 Promoting activity and improving experience

Tackling areas of inactivity through a programme of events and activation - from short to long term. Allowing space for pop-up events / installations and temporary uses to activate vacant spaces and help reduce anti-social behaviour.

### 7 Supporting existing communities & businesses to thrive

The Masterplan will retain the existing economic heart of the neighbourhood and look to help it grow and thrive by improving links into the area and the interfaces with its surrounding influence areas e.g Bon Accord Centre, Bradford Works etc, encouraging the re-use of vacant buildings and identifying areas for potential future development to attract more investment into the area.



Series of precedent images illustrating the type of projects.

Illustrative concept plan showing how the Masterplan objectives could be met



# The George Street Masterplan

- George Street masterplan boundary
- Places, streets & spaces**
- Existing road network
- Streets transformed for people with service access only for vehicles
- Streets to be enhanced
- Improved National Cycle Network route
- Improved links between NESCOL & Robert Gordon's College to George Street
- Existing pedestrian links
- Potential New Public Space
- Improved junctions through public realm and pedestrian crossing upgrades
- Landmarks & buildings**
- Existing Buildings
- Opportunity to create a landmark destination as part of retained Norco House
- ✳ Opportunity for key building as part of future re-development
- Improvements to existing shop frontages
- Potential to create new development frontages that address the street
- Potential to animate street through active ground floor uses
- Opportunity to improve the Bon Accord Centre's entrance
- Activate & animate dead frontages
- Opportunity to reconfigure and enhance edges of NESCOL campus
- Land uses**
- Potential redevelopment opportunity areas
- Development opportunity sites beyond the masterplan boundary



Figure 12 - The George Street Masterplan

## The Masterplan

The Masterplan proposes physical changes to improve opportunities for active travel, for greener more sustainable environments, introduce changes that support the vibrancy and vitality of the neighbourhood whilst improving connections with the city centre.

George Street is, however, a complex existing urban environment and the scope of physical change must be carefully managed and delivered alongside the existing requirements of local businesses and residents as well as being sensitive to its existing context.

However, a clear step-change in the quality and function of the environment, to support the vitality of existing businesses, residents and the communities of George Street is needed. The uplift in place quality will also support longer term redevelopment opportunities when they become available and help to tackle the issues raised by local people around anti-social behaviour, underutilised spaces and neglect.

The Masterplan will be used as a tool going forward to co-ordinate and prioritise key projects (both short & long term) and areas of focus for the neighbourhood. The Council has a clear ambition for the area and it will seek to both deliver and support projects alongside existing stakeholders and those seeking to take forward future development. Delivery of the Masterplan will require a co-ordinated and a collaborative approach across public and private sector organisations.

There are 3 components to the Masterplan that combine to deliver on its Vision and objectives.

### Places, Streets and Spaces

- Streets that connect to existing green spaces beyond the Masterplan boundary such as Cruyff Court and Union Terrace Gardens will be improved to better facilitate active travel;
- The Masterplan area will become an active travel friendly neighbourhood throughout, promoting safer and more equitable sustainable journeys;
- Reconfiguring current street function and traffic direction to better manage traffic flows through the area and creating greater opportunities for active travel;
- Opportunity to create a new public, flexible space for the neighbourhood focused around Norco House / St. Andrew St / Loch Street and Berry Street. The space currently used for existing road infrastructure around Berry Street / Loch St could be reduced and given over to public space, as part of a co-ordinated, Masterplan wide updated street network. There is opportunity for a public space here to be further expanded if potential future proposals on the Norco House site explore an integrated public space. Both capable of creating new spaces where there are currently none; and
- Creating improved arrival gateways into the George Street neighbourhood and within key areas within the Masterplan, through public realm enhancements, such as clear directional and welcome signage, improved crossings at key junctions and, carriageway amendments/ revision to width.

### Landmarks & Buildings

- The Masterplan supports a landmark building for George Street through re-working of the original 1966 Norco House building. It could be brought back to active use through re-purposing and re-development, creating a distinctive landmark destination.
- Quality shop front improvements along George Street can create a more co-ordinated townscape that still retains the individual character of shops whilst also creating a degree of consistency to reduce the disjointed appearance along certain units. This will also involve clearing other redundant clutter such as satellite dishes and unnecessary street signage;
- The north elevation of the Bon Accord Centre and multi storey car park (MSCP) are a key interface with the neighbourhood and should be enhanced. Interventions should look to visually screen / animate the building, improve pedestrian entrances and animate / activate Loch Street.
- There is also the opportunity to introduce a better balance between car parking and available green / multi-functional space within the NESCOL site and creating new connections through the site, offering more space to students and visitors as well as enhancing the environment along Loch Street.

### Land Uses

- George Street is a compact, historic environment and there are potential areas within the Masterplan that could be brought forward for re-purposing or re-development. These locations currently comprise low quality built form, inefficient use of space and vacant buildings.
- A longer term opportunity area exists to the west of George Street, between St. Andrew Street and the Bon Accord Centre. The Council has no direct land ownership in this area however the Masterplan supports opportunities to see the area improved through mixed use and residential redevelopment of vacant buildings and underutilised parcels of land;
- The Masterplan will support longer term, redevelopment around Loch Street (adjacent to the NESCOL) and the Spring Garden Charlotte Street area, should the market come forward with proposals. There is potential to improve the quality of the built form and public realm environment around the college and opportunity sites can help to achieve this. This could involve short term improvements to existing buildings, or longer term re-development should opportunities become available.

## Places, Streets & Spaces - public realm

The consistency and quality of the public realm across the Masterplan are vital to the ongoing vitality of the neighbourhood. All the streets across George Street will have an important role in fulfilling the Masterplan Vision and objectives.

The streets are one of the main areas of influence and opportunity for change within the historic grid network of George Street and will provide a better balance between accessibility of vehicles and active travel.

There are things that all streets should look to achieve that is focused around people and which includes;

- Creating greener streets;
- Character;
- Environment;
- Sense of place; and
- Improving signage and identity.

The Masterplan proposes the improvement of streets so that they provide an enhanced public realm function. This section describes the various roles that each street within the Masterplan will play in doing so, creating higher quality environments for people to enjoy.

### Creating greener streets

One of the key aspirations for all of the streets for the Masterplan is to increase the provision of green infrastructure and biodiversity gains through a variety of interventions. An evolution from grey to green, creating green infrastructure. The climate change and biodiversity loss crises are inextricably linked and one cannot be addressed without tackling the other.

Establishing a strong network of green streets will help connect to wider open space assets including Union Terrace Gardens, St Nicolas Gardens and the wider City Centre streetscape projects.

The suitability of each intervention will be determined though more detailed design testing but could consist of a combination of new features including;

- New tree planting
- Sustainable Drainage Systems (SuDs) such as rain gardens
- Retrofit features to existing structures, for example green wall features along existing dead frontages
- Mini habitats (for example bee/bug boxes, living roofs, hibernacula and nest boxes)
- Street greening features such as shrub planting, wild-flowers, edibles and sensory planting; and
- Green and living walls and roofscapes.

The projects can be delivered both in the short term and long term and could consist of modest to more fundamental approaches which could include carriageway re-configuration. There will need to be balance between achieving green infrastructure within the existing street sections and the impact on movement patterns across the Masterplan and wider area.



An example of how green infrastructure can be implemented within existing streets.

### Bringing colour and character

Streets across the Masterplan will facilitate opportunities to introduce elements of colour to enrich the character of the area, creating a unique sense of place. A co-ordinated colour palette for the neighbourhood will allow for both continuity and colour variation across each street depending upon location and activity levels. George Street itself could become the recognisable centrepiece, showcasing character, through colour.

Combinations of colour features could include;

- Co-ordinated vibrant street furniture
- Updated and improved material palette
- Introducing bold graphics and colour bursts to spill out spaces and buildings
- Extension to the NuArt street art festival further into areas of George Street
- Bringing in bespoke lighting to areas and buildings to help illuminate.

The introduction of colour and contrast must be carefully balanced, to ensure the needs and requirements of different user groups are fully considered. As the Masterplan moves forward, the needs of those with sensory and neurodiverse sensitivities will continue to be consulted, to help further refine and develop opportunities for colour within the streetscape.

### Using more signage & interpretation

Developing a co-ordinated signage and wayfinding strategy can help to further identify George Street as a distinctive place within the city centre and improve its links with its neighbouring parts of the city. Welcome and directional signage at entry points to the neighbourhood can define arrival points and create more visibility. Interpretative and information signage can help to inform people of the place and its local features.

Examples of how new features such as planting, signage, materials, seating and colourful installations can add colour and vibrancy to existing street settings



Examples of how a variety of changes can enliven streets, from material changes, biodiversity planting to playful ways of bringing colour to the street

Each street within the Masterplan will have an important role to play in providing enhanced public realm provision whilst also ensuring its proposed movement functions. Each street can fall broadly into the following typologies in terms of their public realm role;

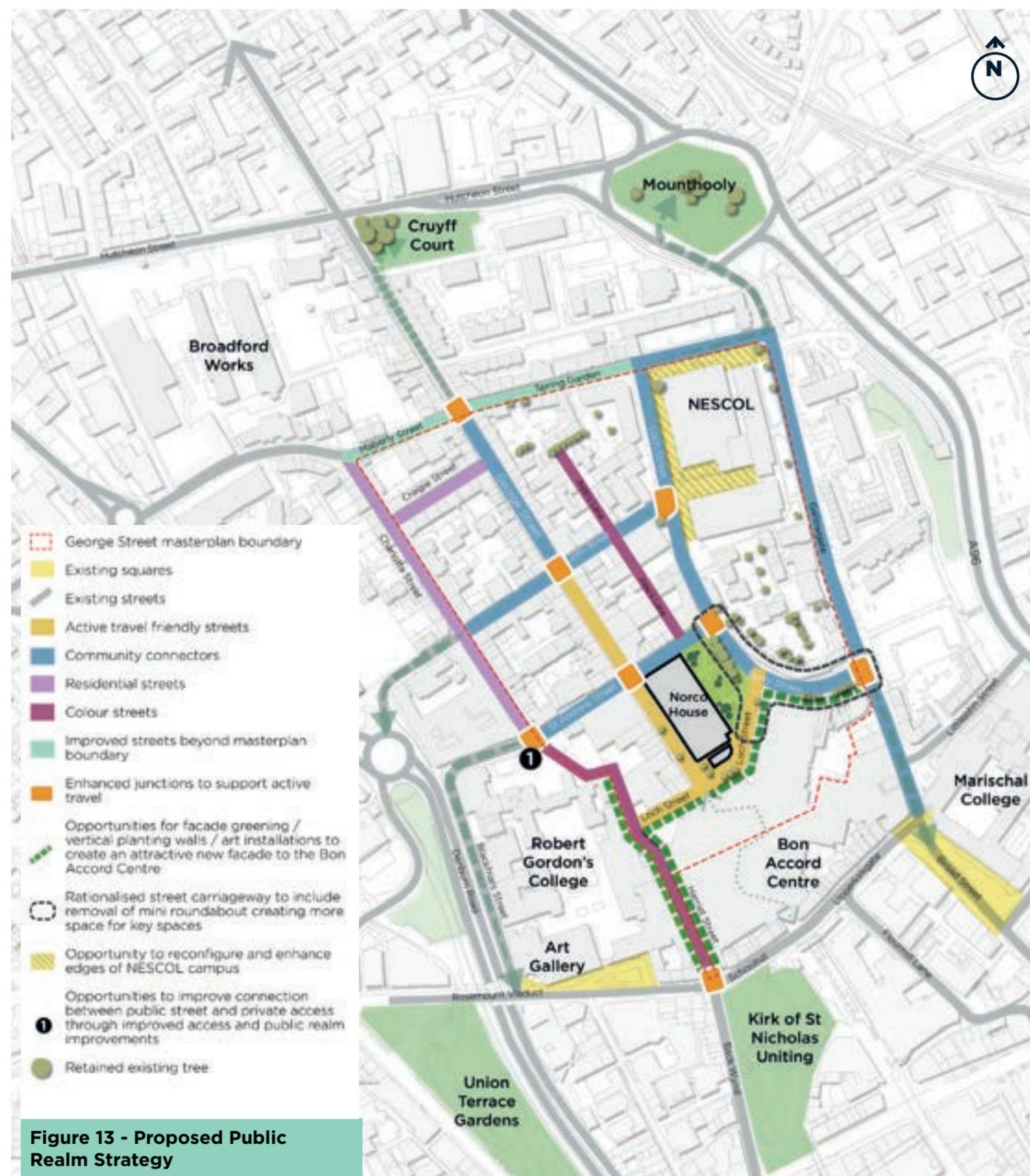
**Streets transformed for pedestrians** - will promote active travel improvements that is suitable to the existing context and that create a significant amount of space for people within the street section. They will allow people to move and dwell along the distinctive and neighbourhood defining George Street corridor. Creating more space will also support space for outdoor seating, bio-retention verges and/or rain gardens with pollinator friendly perennial planting, semi-mature tree planting of pollution-tolerant species.

Interventions could be lower cost and initially more modest which may initially begin as temporary installations. Examples include: parklets, flexible market spaces / exhibition space, multi-purpose moveable planters and street furniture, and/or playful artworks and road markings.

**Community connectors** - important streets that provide onward movements to neighbouring areas and as a result public realm interventions must work to support all modes of travel. Improvements will include an increase in pedestrian space, with cycling accommodated via a combination of segregated lanes and on-road cycling, depending on the unique characteristics and requirements of each street. Some of these streets will see reconfiguration to create improved footway provision, a mixture of semi-mature tree planting with below ground soil cells and/ or standard trees in large planters (either on both or single side of the street), accessible rain gardens with sensory pollinator planting and planted green verges adjacent to roads.

**Residential streets** - quieter streets with predominantly residential properties along them and which will be enhanced to improve the quality of life for residents. These streets will see improved footways, traffic calming, opportunities for informal play, screened bin storage areas, tree planting, integrated timber bench seating, grow spaces and planters and colourful and sensory perennial planting.

**Colour streets** - those streets that are narrow and may not be able to accommodate the same level of soft landscape features as others but can still be transformed. Public realm interventions along Colour Streets will include green walls, colourful planters, building up lighters, lighting nets and seating.



## Places, Streets & Spaces- movement and access

A primary focus of the Masterplan is to ensure that people travel **‘to’** George Street and **‘not through’**. To achieve this, streets must offer a better balance between experience, connectivity and functionality.

The street hierarchy, informed by the Urban Realm Design Guide 2021, will inform future public realm design and movement function detailed in later sections. It will ensure that all streets across the neighbourhood promote active and sustainable movement, in accordance with National policy.

### Street Hierarchy

The existing streets are identified, as part of the Masterplan strategy, into the following three categories:

**Primary Streets** - have a significant influence over the area in terms of visual impact, modal offering and built status. George Street, and St. Andrew Street have been categorised as primary routes, given their significant role, or potential, in active and sustainable travel modes and ‘place’ quality.

These streets are framed by larger buildings and businesses, creating a busier environment than secondary and tertiary streets and will carry a variety of users. They also convey a greater ‘sense of place’.

**Secondary Streets** - provide routes for vehicles across the area as well as active travel, but are not defined by the same levels of landuse activity. Spring Garden, Loch Street, Maberley Street and John Street are identified as secondary streets due to their function, carriageway narrowing and surrounding built character.

**Tertiary Streets** - are quieter in nature by comparison, to secondary and primary streets. Across George Street, tertiary routes are profiled by their narrower street width and quieter levels of vehicular and pedestrian activity. Jopp’s Lane, Harriet Street, Craigie Street and Charlotte Street as well as lower sections of Loch Street have been categorised as tertiary streets as they are more enclosed. These streets offer points of intrigue off busier routes, but will require environmental improvements to ensure they are transformed into pleasant, walkable streets.

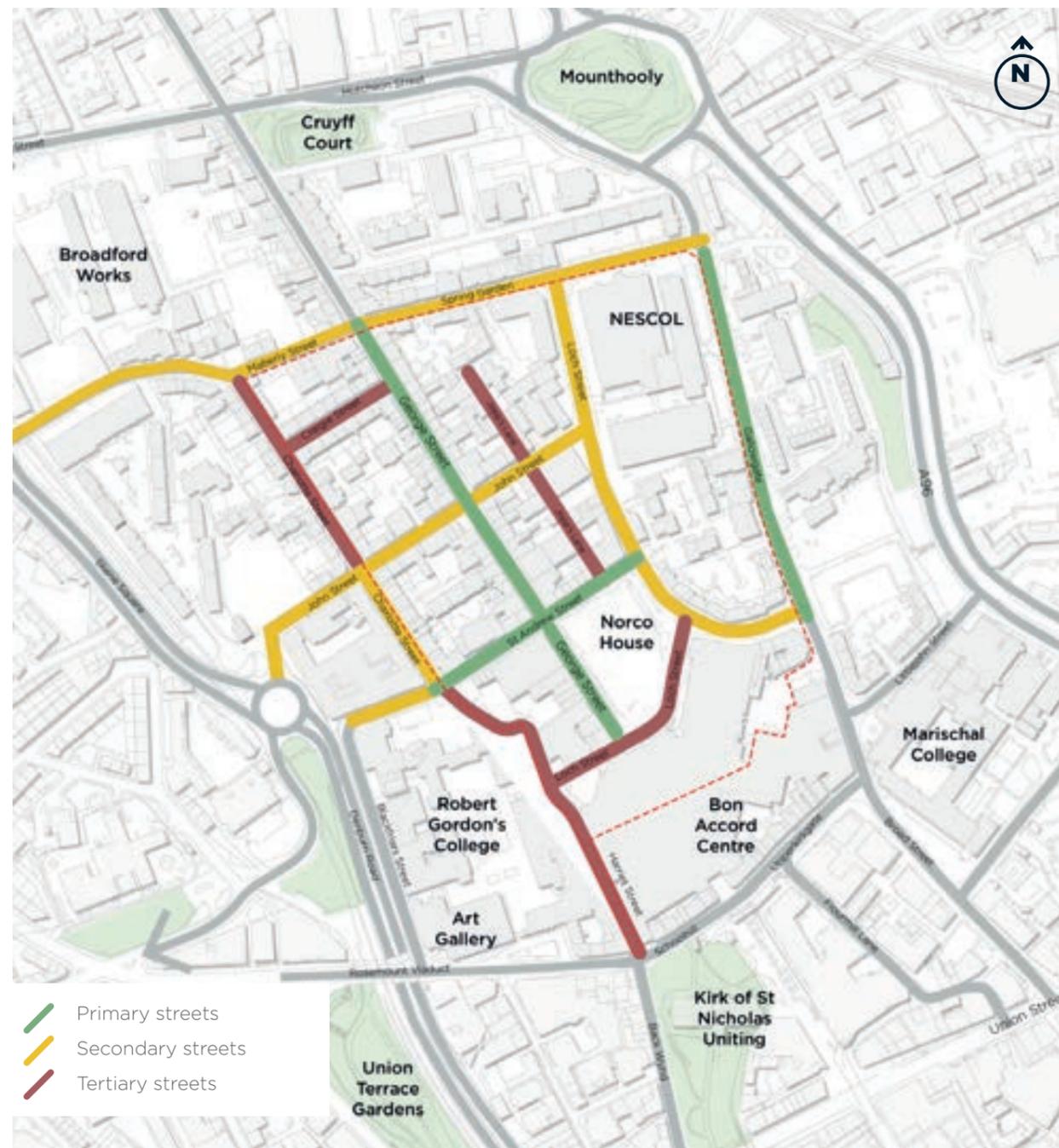
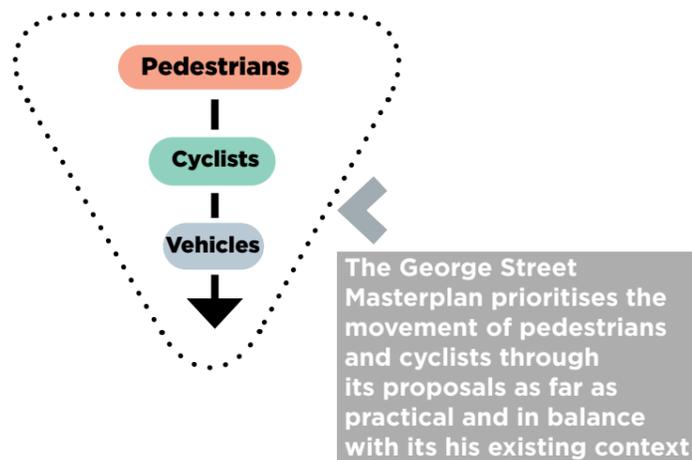


Figure 14 - Proposed Street Hierarchy

## Cycle Accessibility

All streets across the Masterplan area should promote active and sustainable travel. The extent of cyclist specific infrastructure that can be achieved must also be considered in balance with the Vision and objectives of the Masterplan and, as an appropriate response to the historic street grid and structure of the environment. The proposed improvements to the Masterplan street network will include traffic management measures to help reduce the volume and speed of traffic through the area, allowing the majority of streets to be conducive to on-road cycling, per Cycling by Design. Where speeds and flows may limit the level of service that on-road cycling would provide, segregated facilities are proposed.

Currently there are no formally segregated cycle routes through the area. The Masterplan aims to improve cycle accessibility by:

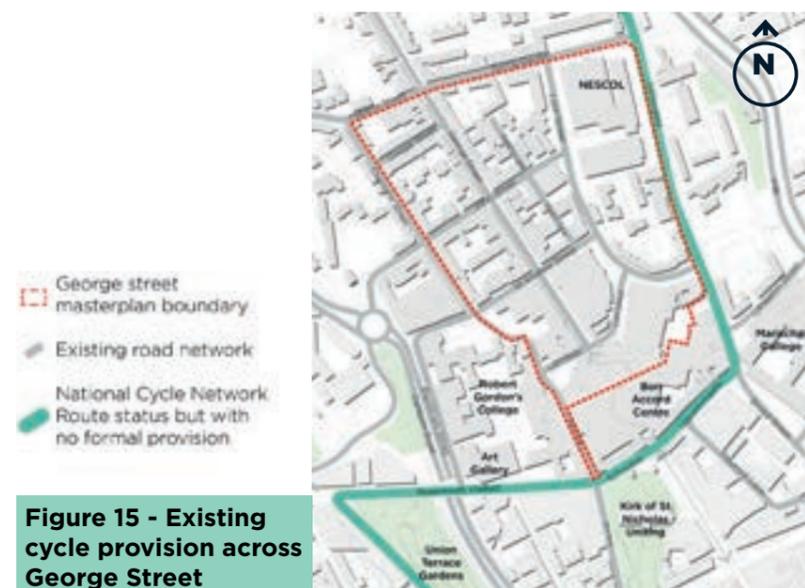
- providing a segregated cycle lane along Gallowgate emphasising the NCN route and its connection into the recently lined Upperkirkgate and Schoolhill;
- providing a segregated cycle lane to part of Spring Garden and Loch St, allowing for segregated cycle access between the NCN Route along Gallowgate and NESCOL, which is a destination generating cycle trips. This would consist of a 2.0m wide segregated cycle lane both east and westbound between the junction with Gallowgate and the junction with Loch Street;
- the remaining streets will be cycle friendly by creating safer and accessible 'mixed streets' across the Masterplan.

## Mixed Streets

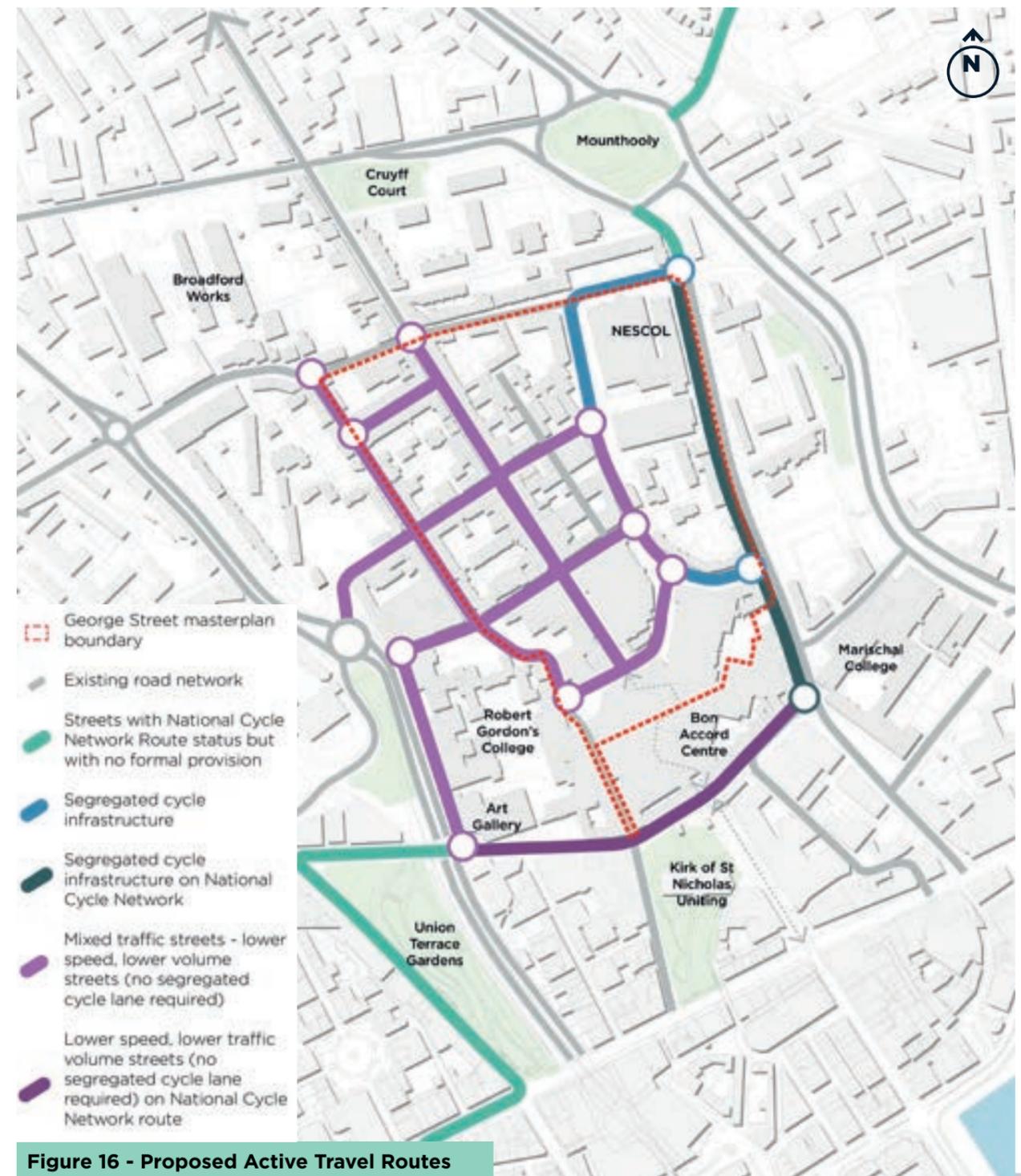
These streets will offer a 'medium level of service' (term used to describe criteria within Cycling By Design) and will require no additional lining within the carriageway. The proposed designs for each, which consist of a reduction in lane widths and traffic calming measures, will support cyclists to feel comfortable taking up a more positive position within the lane as part of a 20mph, low speed urban environment. In terms of providing a basis behind the decisions taken, Cycling by Design suggests that a Design Review is carried out.

Each street will also look to include cycle parking facilities, cycle delivery loading / unloading and repair points to allow users to carry out any repairs to their bikes.

The ultimate design of segregated routes and mixed traffic streets will be dependent upon more detailed traffic analysis and street design. It will also be designed in line with Cycling by Design 2021 (or subsequent) guidance and the Urban Realm Manual.



**Figure 15 - Existing cycle provision across George Street**



**Figure 16 - Proposed Active Travel Routes**

## Vehicular Movements

Managing vehicle access across the Masterplan area can lead to:

- an improvement in local air quality;
- reduced vehicular conflict for public transport and active modes of travel; and
- create more space within the street for better facilities for public transport, pedestrians, cyclists and spill out space for local businesses.

Each of these combine to create a stronger sense of neighbourhood and, in order to achieve this, a number of changes to the function and direction of traffic along certain streets is proposed.

### Proposed Areas of Change

The main principle for managing vehicular access within George Street is to prioritise access to those who need to enter or exit the neighbourhood (e.g local residents, patrons to local businesses). This principle is complementary to the Council's Roads Hierarchy and City Centre and Beach Masterplan requirements of a place as a destination (a place to travel to) but not a through route.

Robust traffic modelling work has been undertaken to test the extent of through routing across the Masterplan and the proposed changes tested, in order to provide wider benefit to the area and facilitate the ability of the street network to provide more active travel and activity as well as transform the function of George Street itself.

The proposed vehicle movement strategy for the Masterplan is detailed in **figure 19** (overleaf). The following points below (A - G) explain proposed changes to existing vehicle flows across the Masterplan street network and are highlighted in **figure 18**;



### Phase 1

- A.** George Street south-bound to John Street for general traffic with north-bound bus route
- B.** John Street to become one-way west bound from George Street to general traffic with no general traffic entering from Denburn.
- C.** The section of George Street between John Street and St. Andrew Street is to be a service access only route, removing both bus routing and general traffic movement. This will allow for a greater focus on pedestrian and cycle focused movement, promoting active travel in the heart of the neighbourhood and creating a new sense of place.
- D.** Blackfriars Street to be come one-way south bound to general traffic and two-way for bus and taxi east and north-bound
- E.** In the initial stages of development, St. Andrew Street would become one way west bound from George Street to Blackfriars Street for general traffic, whereas bus and taxi routing will remain two way.



Figure 17 - current direction of vehicle flows

### Phase 2

- F.** St. Andrew Street would introduce restrictions to private car traffic (potentially via provision of a bus gate / or signage) between Charlotte Street and Loch Street junctions further reducing the dominance of general traffic movement across the neighbourhood.
- G.** Phase 2 would also restrict general vehicle movement between Loch and Berry Street at the St. Andrew Street junction (potentially via provision of a bus gate / or signage). General vehicle access on Berry Street would be allowed for access to the Bon Accord Centre car parking only and service access.

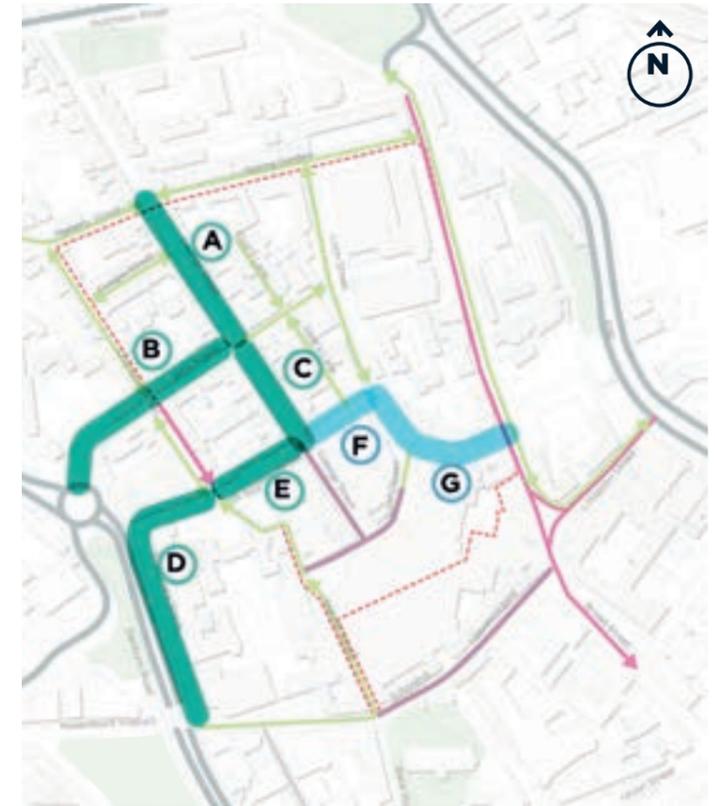


Figure 18 - Streets within the Masterplan where a change to the traffic flow is proposed

## Proposed Vehicular Movements

The proposed vehicular movement network across George Street comprises:

### 1. Two Way Streets

Streets that allow for vehicle movements in both directions. They are located to the edges of the neighbourhood and they provide easy access to the surrounding strategic routes.

### 2. One Way Streets

These streets provide access along sections of George Street, John Street, Jopp's Lane and along Harriet Street. Vehicles are directed back onto two way streets. Their one way movements through routing and create a lower car environment around the George Street, John Street, St. Andrew Street core of the neighbourhood.

### 3. Pedestrian Priority / Service Access Streets

Streets open to vehicle access and which allow local users and patrons access to the neighbourhood. These streets will be enhanced to promote greater pedestrian and active travel movements and designed in a manner which manages traffic speeds.

### 4. Car Park Access Only

Berry Street must remain accessible to vehicles to allow access to the Bon Accord Centre multi storey car park (MSCP). It will provide access from Gallowgate and Loch Street and egress from the MSCP onto Berry Street.

### 5. Bus Servicing

Phase 1 of the vehicular strategy looks to work with the existing bus routes as far as possible. The only alteration to existing routing would be along the section of George Street between John Street and St. Andrew Street, where there are currently no bus stops. Removal of bus movement from this short section will allow for more active travel and spill out space to create a different environment.

Phase 2 of the strategy would take a more radical approach, reducing the St. Andrew Street carriageway, between Charlotte Street and Jopp's Lane to bus access only. The lower section of Loch Street to Berry Street would also reduce to bus access only too. Cars entering St. Andrew Street would be redirected northbound on Jopp's Lane to continue their journey. This bus priority could be managed through restriction for example bus gates or appropriate signage, subject to further detailed design following implementation of Phase 1.

### Impacts on through routing

A detailed traffic modelling exercise has been undertaken to understand the movement strategy impacts and benefits. The results of the modelling have demonstrated that the proposed measures would introduce changes that help deliver the Masterplan ambitions through:

- Traffic management proposals that reduce opportunities for through-traffic in the area (circa 21% reduction in through-trips across the day) while maintaining and, in some cases, enhancing accessibility for all modes;
- Displacing traffic from local roads in the core of the study area onto streets at the top of the revised Aberdeen Roads Hierarchy which are of a standard and a capacity appropriate for higher volumes of traffic, while minimising the overall impacts on the network as a whole; and
- Balancing the accessibility requirements across different modes.

The modelling has demonstrate that the proposals can deliver significant positive change through reduced traffic flows on key streets that are central to the Masterplan objectives and priority projects.



Figure 19 - Proposed Phase 1 movement flows

## **4. Transformational Projects**

# 4. Transformational Projects

◀ Transformational Project Summaries

## Delivering change

Transformational projects are those areas of focused intervention within the Masterplan that can crystallise the Vision and consultation findings into tangible actions / projects that can be taken forward and developed further.

There are 6 transformational 'place' projects identified in the Masterplan which are centred around physical change and regeneration:

- Enhancing and modernising the streetscape including enhancements to the public realm, de-cluttering and improved footways, places to dwell and spaces to host street events
- Improve safety and crime rates in the area, through enhancements to the public safety such as more CCTV, better street lighting and greater levels of activity (through different uses and types of spaces) to improve the sense of safety both during day and evening, creating an identity through lighting emphasising 'place'
- Introducing space to support meanwhile uses and street greening, to introduce more activity and animation to the area with temporary seating, planting features, signage and wayfinding
- Improve the pedestrian experience through enhanced materials, lighting, wayfinding, pedestrian prioritisation measures and managed streets that encourage active travel and discourage through-routing
- To make more of the Bon Accord Centre and Norco House area of the Masterplan, to create more activity and community space

In addition there are also opportunities for local communities and stakeholders to contribute to, influence and assist in the future delivery of these projects. Community and stakeholder group capacity building will play an essential role in both helping to shape these projects as they move forward and to ensure that projects are supported and informed by local residents, businesses and institutions within the Masterplan, as the main custodians and users of the George Street neighbourhood.



# Transformational Project 1

## Norco Place

This section sets out potential, illustrative future proposals for Norco House and its surrounding area which could become more widely recognised as 'Norco Place'. It explores two potential development scenarios each with a different approach to future development use and provision of space to provide for the wider area.

These scenarios are not ranked in order of preference but explore the art of the possible whilst also demonstrating how each work with the principles established within the Masterplan. It is hoped that these help to re-frame the potential of the building and identify new potential design responses, uses and public realm opportunities which re-establish the area as an important city wide anchor and diverse neighbourhood.

### Scale of change for Norco House

One of the scenarios assumes the retention of the Norco House building, retaining the striking and distinctive brutalist form of the building and its distinctive elevations. This option works to reactivate the building and assumes that the current floorplate and column grid remain in situ.

There may be potential to retain the brutalist exterior of the building but strip back the interior floor plates to offer a greater degree of flexibility

and accommodation of uses. This would require further design testing and development and would be subject to more detailed surveys. Historic Environment Scotland have previously been asked to consider this structure for listing. Following their assessment it was found that the surviving two principal elevations (to George Street and to St. Andrew Street) met the criteria of special architectural or historic interest. However, the decision was not to list the building because of ongoing development proposals.

Each scenario also considers a variety of potential future land uses. Options that focus on the retention of the Norco House building are focused on a mixed use offer, providing opportunity for cultural, leisure and community uses given the depth and design characteristics of the building in its current form. The option contributes to the economic sustainability of the neighbourhood accommodating a variety of people and will be diverse in its property type, avoiding mono-type development.

### A Retrofit First approach To Norco House

Retention of Norco House allows for a retrofit first approach in response to resource pressures and more sustainable approaches to the re-use and adaptation of city centre environments. Retrofit can minimise the carbon produced through demolition, landfill and extraction of new materials for new development.

Historic Environment Scotland's (HES) National policy 2023 'Our Place, Our Future' provides three priorities 1) Delivering the transition to net zero, 2) Empowering resilient and inclusive communities and places and 3) Building a wellbeing economy, which emphasise that maintenance, reuse and adaptation of our historic environment can make an important contribution in preventing waste and reducing carbon emissions.

Norco House in particular is a building with high embedded carbon due to its concrete construction. Demolition of this building, the removal and processing of the waste and the redevelopment of the site would produce more carbon emissions.

When looking at design options for the retrofit of Norco House these are some of the main challenges that would need to be addressed:

- Deep floor plan presents issues with the

amount of daylight that can penetrate the building, hence the suggestion to retain the original building;

- Concrete facade with minimal windows;
- Inactive frontages at ground floor; and
- Internal and external condition of the building has not been surveyed.

Yet the building also has positive qualities that will allow it to be successfully refurbished:

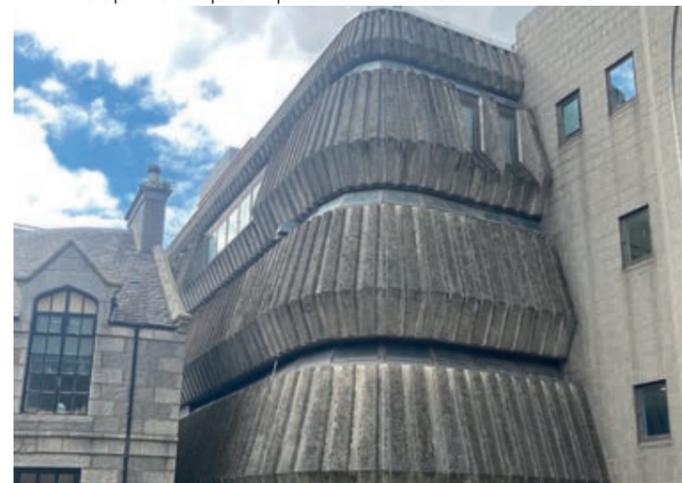
- It has a regular and widely-spaced structural grid allowing for internal efficiencies;
- Generous floor-to-ceiling heights;
- Potential for high live retail floor loadings allowing for additional floors to be added.

There is potential to totally re-imagine the future use of Norco House, diversifying its offer to the area and wider City. The retrofit and reactivation of this building being a statement of success and an example of how the challenges of reducing, reusing and recycling existing assets in innovative ways can be achieved.

The scenarios shown are indicative and would be subject to further design development, detailed assessments of the building conditions, viability testing and collaboration with the building owner.



Figure 20 - Location of Norco House



Norco House in its current state

## Scenario 1 - Renovation, rationalisation and re-use

This scenario explores the potential to demolish the later Norco House extension, retaining and refurbishing the distinctive 1960s Norco House.

The demolition makes way for a new 'Norco Place' urban civic space. A new glazed facade to the east of the building allows a greater amount of natural light into the building whilst providing an animated new, mixed use address onto the square.

Historic Environment Scotland welcome the consideration given in the assessment around the future of Norco House, with the George Street and St. Andrew Street elevations recognised as an environment asset and of townscape character / interest, and that the loss of the building, specifically its noteworthy two elevations, would constitute a 'negative effect to the cultural heritage of the area'.

### Development Approach:

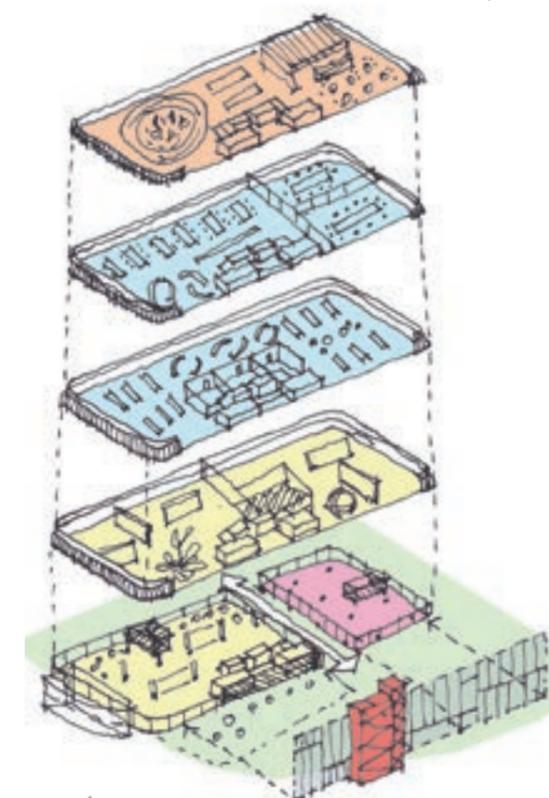
1. Norco House 1980's extension demolished;
2. Retained and refurbished Norco House to accommodate new ground floor and upper floor uses;
3. Demolition of the extension allows for more natural light to infiltrate the floor plan and allow for more flexibility of use;
4. A new pocket park, Norco Place, providing new residential and public amenity along St. Andrew Street and Loch Street. There is potential for this to become a sculpture park, outdoor exhibition space or more flexible uses civic space;
5. Appropriate new uses west of George Street could comprise of ground floor retail, food and beverage and commercial spaces with residential above activating currently quiet and uninviting sections of Loch Street and Crooked Lane;
6. Existing road infrastructure along Loch Street re-designed to create larger pedestrian space and pocket park;



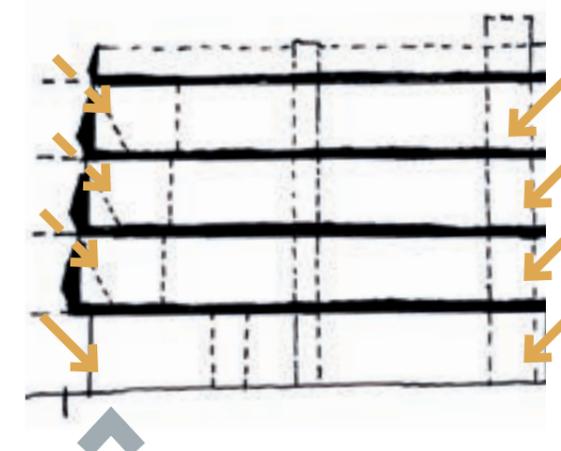
7. Alteration to elevation of Bon Accord Centre along Loch Street with potential for food and beverage units facing onto Norco Place at the ground floor.
8. Facade improvements above could include green walls, re-cladding, feature lighting or murals/ art work to bring colour to the structure;
9. St. Andrew Street streetscape improvements integrated with Norco Place
10. Sub station retained on Crooked Lane who's relocation is feasible;

- Existing buildings
- Retained buildings
- Proposed new development
- Refurbished / re-purposed
- Enhanced public realm
- Pocket Park

Illustrative layout showing how Norco House could be retained and brought back to life next to a new civic space



Flexible structure and column spacing allows for many different uses throughout the building.



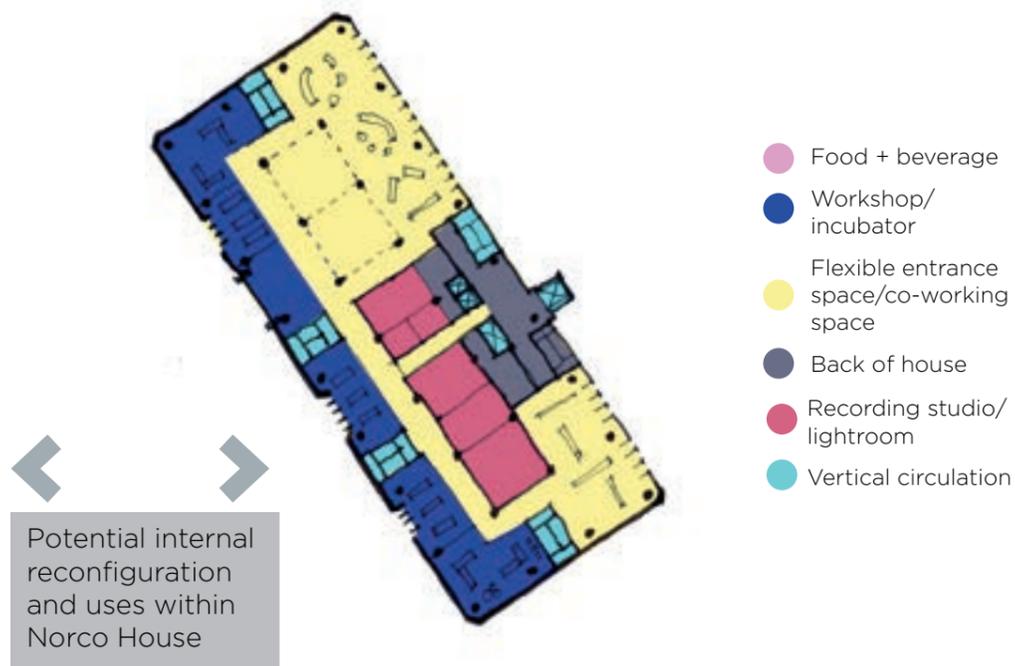
Demolition of 1980s extension allows for more natural light deeper into the plan. More natural light will allow for a greater flexibility in use.

## Scenario 1 - Norco House potential uses



### Ground floor plan

The proposed ground floor looks to accommodate a number of different occupants and users. The open plan and large span between columns allows for the plan to be split up for different functions. The central yellow area is accessed from George Street and provides access to the lift core to the uses above. The areas in light pink could accommodate food and beverage operators bringing day and evening activity to George Street. The dark blue spaces could be flexible workshop spaces for the community, entrepreneurs and start ups. Areas around the lift core and stair cores can act as back of house storage and plant for the different occupiers.



Potential internal reconfiguration and uses within Norco House

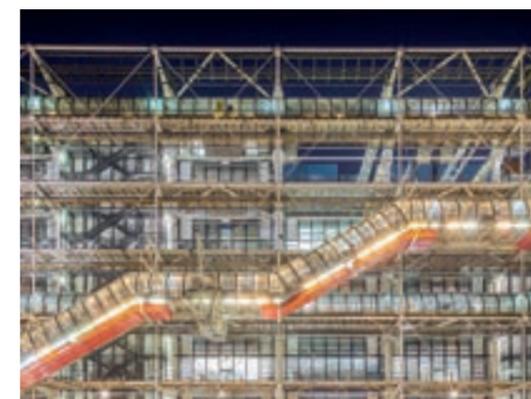
### Typical upper floor plans

The upper floors separate the uses based upon the amount of natural light that penetrates into the floorplan. The existing facade to George Street has mostly clerestory glazing. These locations could be occupied by focused workspaces or incubator workshops that don't require large amounts of natural light and views. The middle of the plan, with the lowest levels of natural light, could accommodate recording studios, photographic light rooms, storage and plant rooms. The new glazed east facing facade could accommodate open plan co-working spaces, meeting rooms and cultural functions (e.g galleries / exhibitions) etc.

There are four lift cores in the existing building and this allows the building to be let out to multiple operators on different levels of the building. For example, the external lift onto the park could provide exclusive access to the activities on the roof, including roof top cinema, urban allotments or community performance space.



Examples of how an existing building elevation can be screened and enhanced with bold and contemporary materials



The Pompidou Centre in Paris has a distinctive building facade that looks onto a public space, creating interest and activity



The existing footings and columns of Norco House could be revealed, creating a flowing connection between building and external space

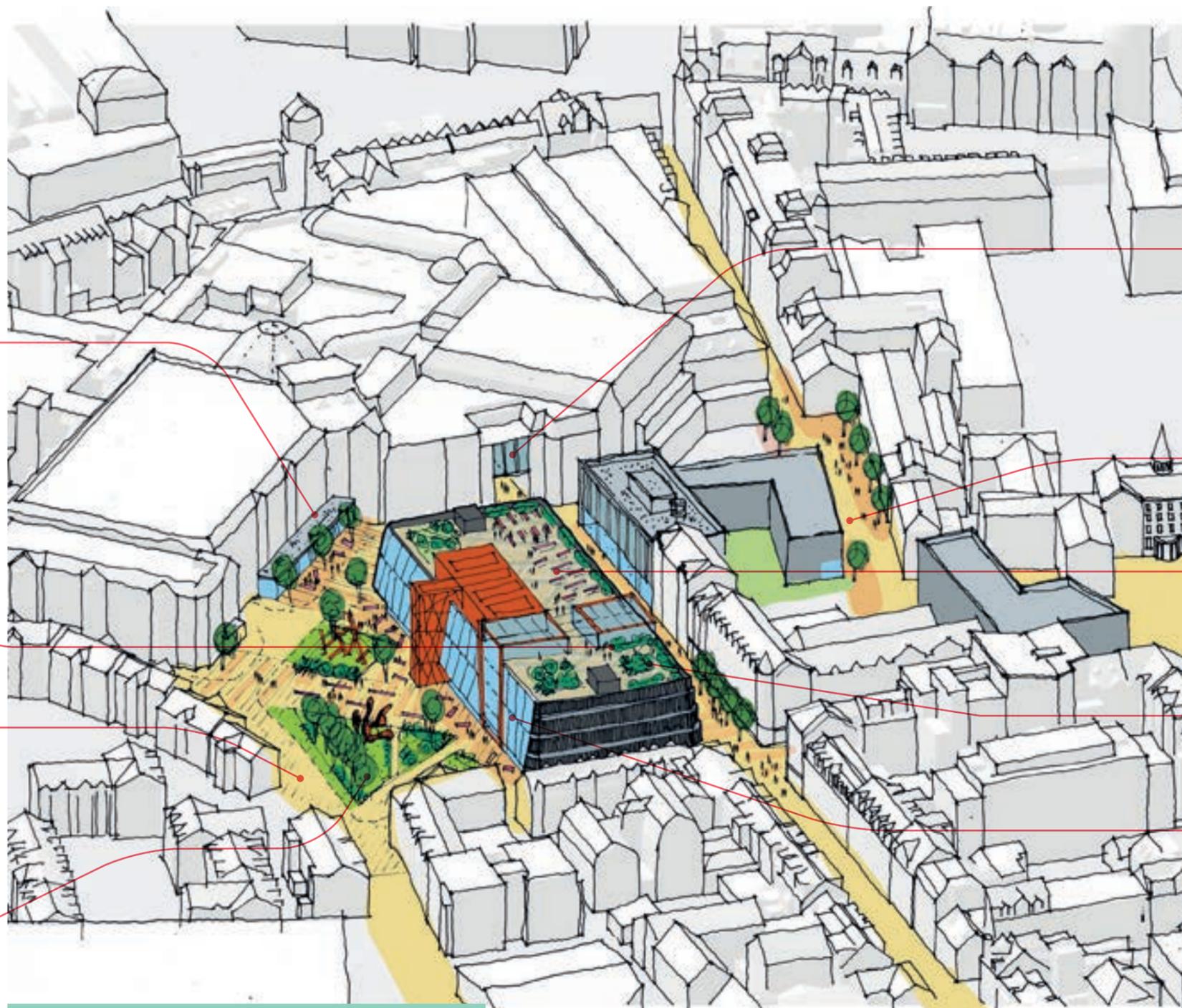
## Scenario 1 - Renovation, rationalisation and re-use

Alteration of Bon Accord Centre's elevation to provide single storey cafe/restaurant/bar units to create animation onto Loch Street

Atrium cut into the existing Norco House structure to provide natural light to illuminate the link between Norco Place and George Street

Existing road infrastructure along Loch Street narrowed to create larger pedestrian space and pocket park. Park could include high quality, indigenous planting that supports a nature rich variety of habitats and pollinator friendly planting.

Norco Place: Ground floor uses from the refurbished Norco House spill out onto the new park and create animation. Potential for sculpture park and external exhibitions



Facade alteration to Bon Accord Centre to allow for better visibility into the shopping centre

Infill mixed use development. Potential for commercial/retail on the ground floor and residential above

New street trees and public realm improvements to Crooked Lane

Roof top public space

Infill mixed use development. Potential for commercial/retail on the ground floor and residential above

Making the most of roof space for solar collection, rainwater collection and sedum roofs

New glazed facade to Norco House onto the Loch Street pocket park. Glazing will allow for more natural daylight into the deep floor plan

### What If!

Norco House was re-imagined as a cultural, entrepreneurial, community and mixed use hub adjacent to a new civic space in the City.

Figure 21 Indicative sketch view of Scenario 1

## Scenario 2 - A Mixed Use Community

This scenario considers the full demolition of Norco House and the Norco House Extension to allow for a new mixed-use development to occupy its place.

The proposal is a mid-rise, mixed-use development accommodating residential, ground floor retail, commercial, food and beverage uses.

A new pocket park sits on the corner of George Street and St. Andrew Street to offer community focused dwell space and that is framed by ground floor active uses. This approach shifts the sense of centre towards the core of the study area and addresses the issues along Loch Street and the interface with the Bon Accord Centre.

As any future proposals were developed, any potential adverse effects on landscape/townscape characteristics should be mitigated through carefully and appropriately designed and sited development which is sympathetic with the existing character of the George Street area.

### Development Approach:

1. Demolition of Norco House and its extension to create new residential led neighbourhood of scale along the southern extent of George Street.
2. Assumes residential development across all floors but could facilitate some non-residential uses on ground floor along George Street to animate the surrounding streets and public realm;
3. Combination of 3-5 storey buildings in locations which create new gateway features as well as respecting the established ridge and shoulder heights of historic buildings along St. Andrew Street;
4. New east - west connection from George Street to Loch Street;
5. Internalised, private green space and residential parking enclosed with residential development and former Soup Kitchen forming part of the setting of the space.
6. Alteration to elevation of the Bon Accord Centre along Loch Street with potential for single storey cafe/restaurant/bar units;
7. Service area entrance to north west corner of the Bon Accord Centre re-located to enter / exit onto Harriet Street



Illustrative layout showing how, if Norco House was demolished, the site could be redeveloped.

- Existing buildings
- Retained buildings
- Proposed new development
- Refurbished / re-purposed
- Enhanced public realm

New build mixed-use development.  
Potential for retail, commercial,  
food and beverage on the ground  
floor. Residential accommodation  
above.

Alteration to elevation of the Bon  
Accord Centre along Loch Street  
with potential for single storey  
cafe/restaurant/bar unit

Active frontage onto  
George Street Square

Existing road infrastructure along  
Loch Street narrowed to create  
larger pedestrian space and pocket  
park



Facade alteration to Bon Accord  
Centre to allow for better visibility  
into the shopping centre

Visual screening of the Bon Accord  
Centre Service area and car park  
ramp

New street trees and public realm  
improvements to Crooked Lane

Front doors onto Crooked Lane to  
provide overlooking

Infill mixed use development.  
Potential for commercial/retail on the  
ground floor and residential above.

George Street Square - Public  
realm pocket park at the corner  
of George Street and St. Andrew  
Street. Active frontages onto the  
space to animate it.

#### What If!

A variety of residential housing  
typologies within a mixed use  
development could be created along  
George Street.

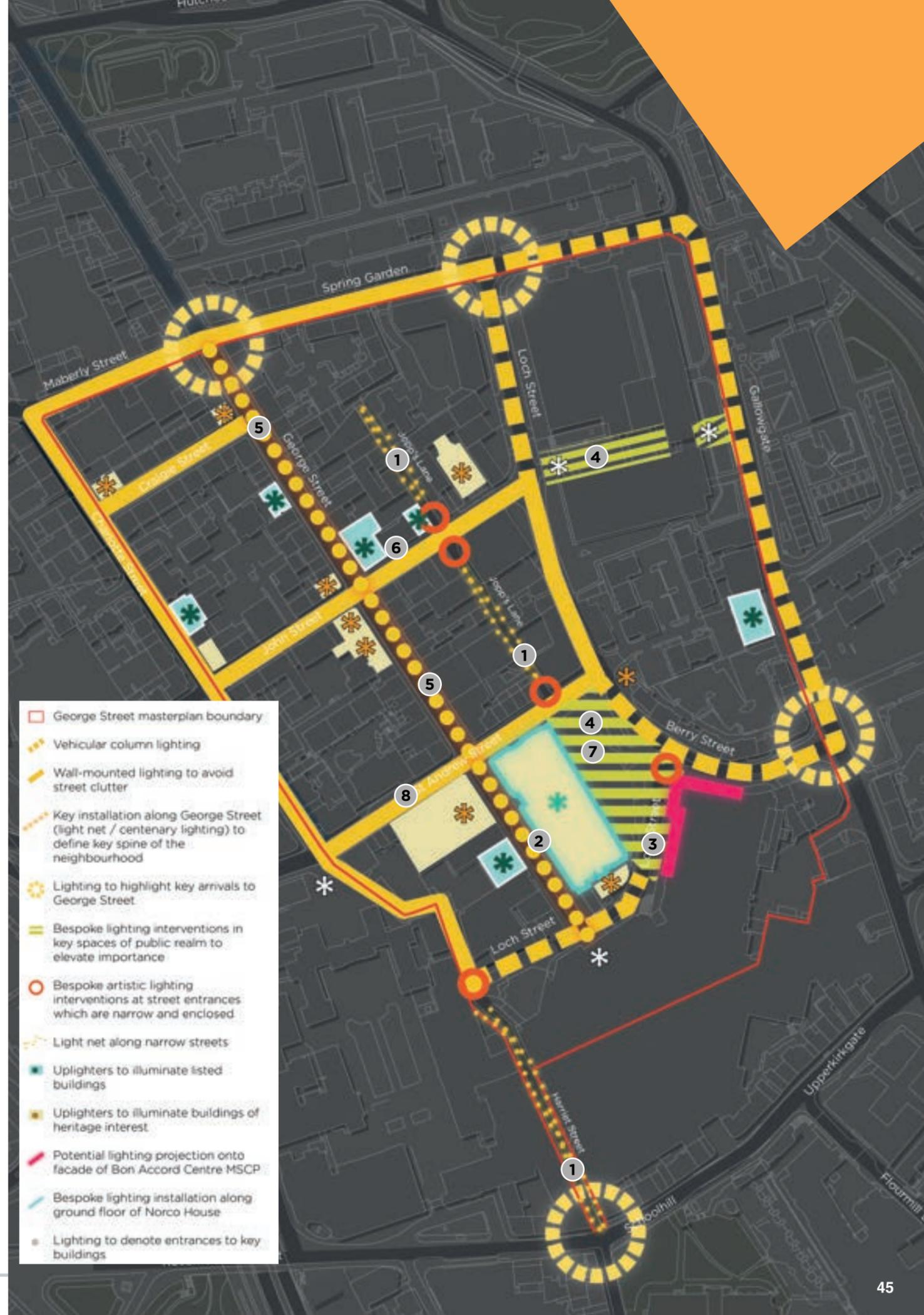
**Figure 22 - Indicative sketch view of Scenario 2**

# Transformational Project 2

## Illumination

Lighting will be used across the public realm to reduce anti-social behaviour, create more welcoming environments and showcase built form character, encouraging more people to visit and use the area in the evening by creating a positive experience. The Masterplan focuses on creating a destination and using lighting for both safety and wayfinding in the following ways;

1. Lighting is responsive to the Urban Realm Design Guide, in a cohesive matte-black, improved as wall mounted lighting, to reduce visual clutter;
2. Along wider street sections, wall mounted lighting may be suitable and to illuminate cycle provision;
3. Introduce a key distinctive light installation along the length of George Street to establish the corridor as the neighbourhood spine. This could include light nets, banner lighting and centenary lighting, subject to further design development;
4. Lighting banners / illuminated signage used at entry points to the Masterplan area from George Street, Harriet Street and Loch Street to denote arrival to the area;
5. In key public spaces there will be bespoke artistic lighting interventions which help create a bespoke identity and sense of arrival to the space;
6. Explore potential bespoke wall mounted lighting which is bold and bright at the entrance to Jopp's Lane and Harriet Street to both identify these as key pedestrian routes of interest as well as a means of inviting people to travel along them; and
7. Lighting used to illuminate buildings of interest across the Masterplan including listed buildings and heritage buildings of interest. Norco House could be illuminated along its ground floor with temporary, changeable light installations as part of an activation strategy for the building.



**Figure 23 - Proposed Illumination Strategy Plan**

# Transformational Project 3

## Streetscape Improvements

The existing street network presents a significant opportunity to redefine the character and vitality of George Street. This will contribute to achieving the overall objectives of the Masterplan and deliver a step change in the quality, sustainability and functionality of the neighbourhood - to support the wellbeing of existing residents, businesses and students within the George Street area.

This section sets out projects for each of the streets listed below which include opportunities for carriageway reconfiguration, increased amounts of street greening, signage, lighting, active travel and dwell space.

The projects identified for the streets below, in order of impact;

- George Street (between St. Andrew Street and Bon Accord Centre)
- George Street (between St. Andrew Street and John Street)
- Harriet Street
- Gallowgate
- George Street (between John Street to Spring Garden)
- Craigie Street
- Loch Street
- Berry Street through to Gallowgate
- St. Andrew Street
- John Street
- Charlotte Street
- Jopp's Lane
- Spring Garden

Although these are illustrative proposals and subject to further design development, testing and consultation, they are intended to provide inspiration for what could be achieved as part of a holistic approach to 'place' across the Masterplan.

In developing streetscape proposals consideration of context and materials to complement the existing desirable place characteristics are guided by the place and movement evaluation as well as space uses, and with material specification from the draft Aberdeen Urban Realm Manual.

The design projects identified in this section have been supported by more detailed traffic modelling to understand the impact of the proposed changes and to ensure that any change or impact experienced is understood. As a result of the proposals, both within and surrounding the Masterplan, are known, reasonably expected and within tolerance to wider movement function across the city.

### Approach to car parking

Each design looks to improve the quality of parking provision across the Masterplan. As a design objective through the next stages of design testing, street design should look to try and retain the current number of car parking spaces across the Masterplan, where possible.

### Approach to Sustainable Urban Drainage (SuDS)

Whilst the historic and built nature of the Masterplan area may limit opportunities for SuDS in some locations, the streetscape improvements look to address surface water drainage and

stormwater issues and provide wider benefits for the community and nature. Measures including rain gardens, planters, pocket park spaces and tree planting can combine as part of SuDS approach. It will be important for the SuDS and natural flood solutions to be incorporated into the design development from the outset and allow public access once implemented.

### Approach to cycling

Alongside environmental improvements to the streetscape, traffic management measures help reduce the volume and speed of traffic through the Masterplan area, making the majority of streets conducive to on-road cycling, as per 'Cycling by Design'. These are identified in the Masterplan as 'mixed streets' and where speeds and flows may limit the level of cycle accessibility, segregated facilities are proposed.

Changes in lane widths will enable cyclists to feel comfortable taking up a more positive position within the carriageway.



### Approach to tree planting across the Masterplan

Street greening is a key aspiration of the Masterplan and street trees will play a fundamental role in achieving this. Where possible, street tree planting is proposed across most streets. At this stage, the current ground conditions and utility servicing are unknown and will require further investigation as designs progress. However, the implementation of new tree planting should look to work with any utility constraints. A preferred hierarchy of tree provision within the streetscape is proposed as suggestions to work around utility constraints.

#### 1. Trees in softscape



Trees within open, unrestricted ground, as far possible, as this is best for tree growth and provides the optimum conditions for healthy tree growth.

#### 2. Trees within underground root systems



As a result of utilities or poor quality soil, root barriers and high volume soil cells may be required to protect adjacent utility services from tree root growth and support healthy growth.

#### 3. Trees in containers above ground



Trees planted in large, moveable containers within the public realm. Can ensure trees feature as part of the streetscape, particularly along narrow streets which lack space and/or flexibility.



Trees in moveable containers can ensure trees feature in the streetscape.

## George Street (St. Andrew Street to Loch Street)

George Street will be an instantly recognisable, vibrant and colourful street that prioritises pedestrian movement and dwell space with activity spilling out from local shops, the Bon Accord Centre and the new Norco Place.

A variety of street tree planting types create contrast to the existing buildings, provide shade and shelter and help to delineate the extents of the service access.

The street is treated as one continuous pedestrian space, with a narrow 3m service strip for intermittent service access for deliveries / drop off.

George Street is the gateway to the neighbourhood from the Bon Accord Centre's northern exit and it will celebrate your arrival to the neighbourhood. Contemporary street furniture which is bold, bright and co-ordinated with signage and eye-catching super graphics help to create character and impact to the street.

Information points, signage, lighting and flexible space for temporary or pop-up events will create a more vibrant, safer and enjoyable space more of the time and change perceptions of the place.

George Street		
Primary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical service access road width	8.50 m	Decreased to 3m
Footpath width	3.50m / 3.80m	Increased to 6.8m / 6m (variable)
On Street parking	N	N
Loading bays	Y	Y
Materiality		
Carriageway	Setts	Granite, tonal setts
Footway	Paving slabs	Granite, tonal setts
Streetscape		
Refuse storage	Y	Y
Street planting	Y	Y - tree planting and rain gardens
Street furniture	Y	Y - various types of fixed and moveable

Table 3 - George Street Details

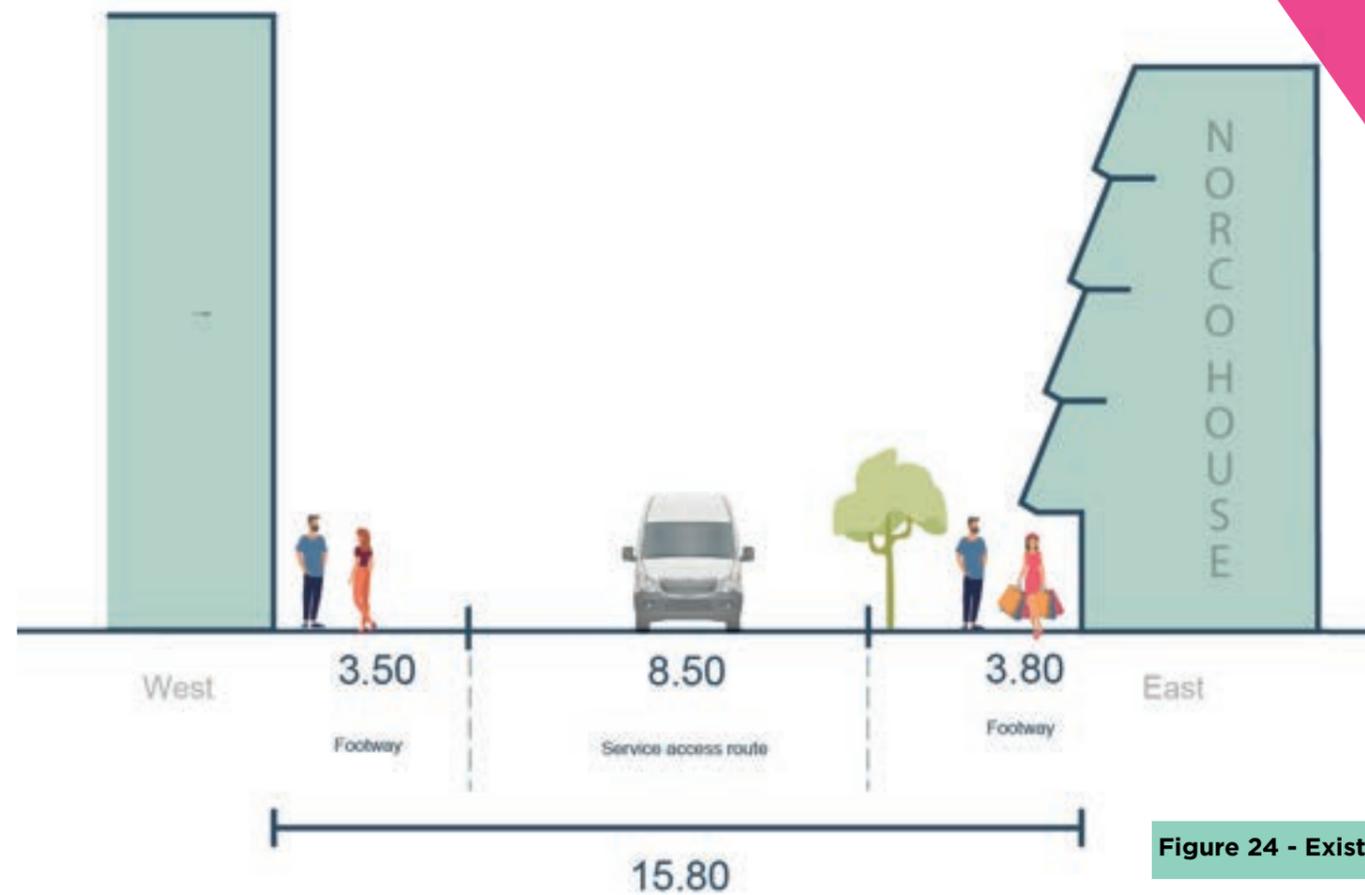


Figure 24 - Existing street section

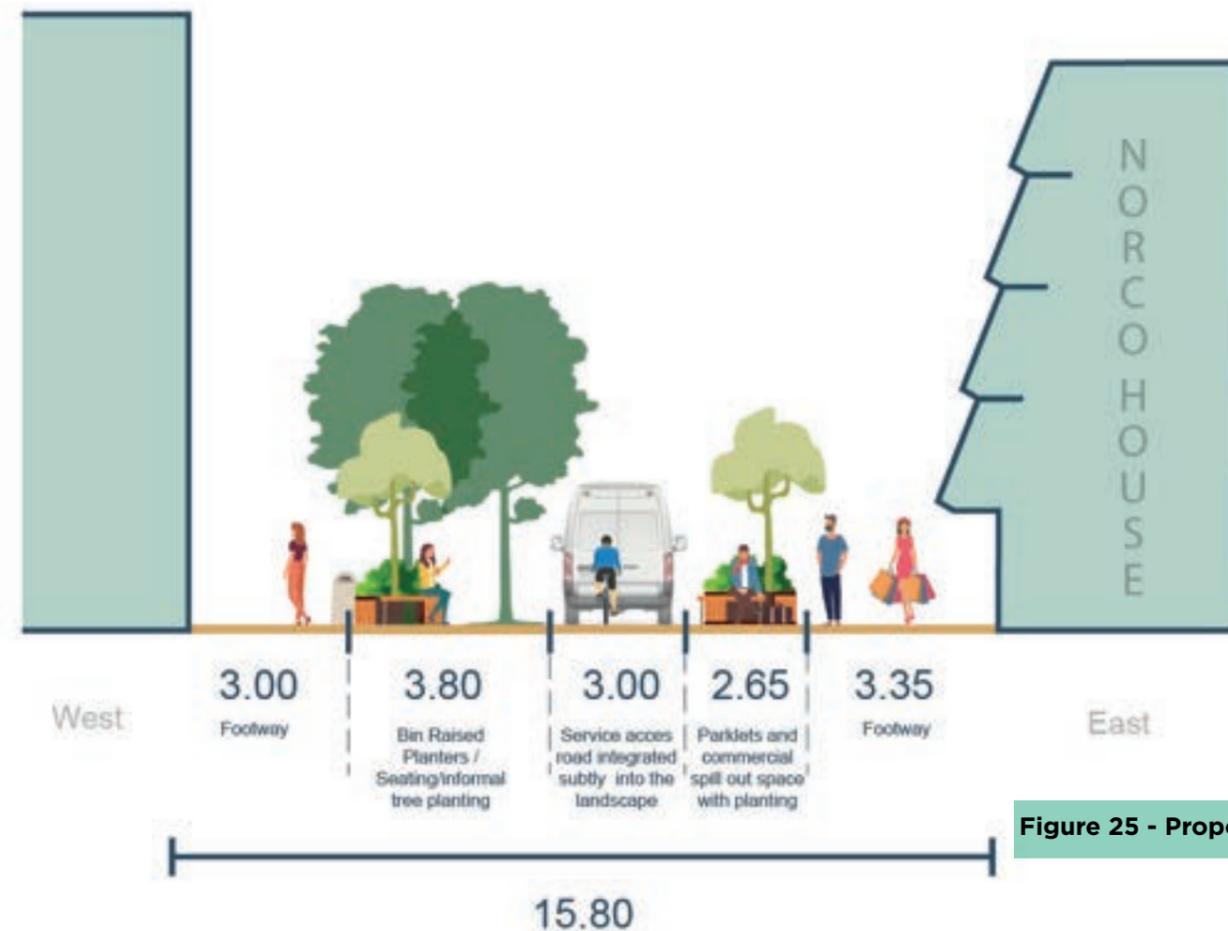


Figure 25 - Proposed street section

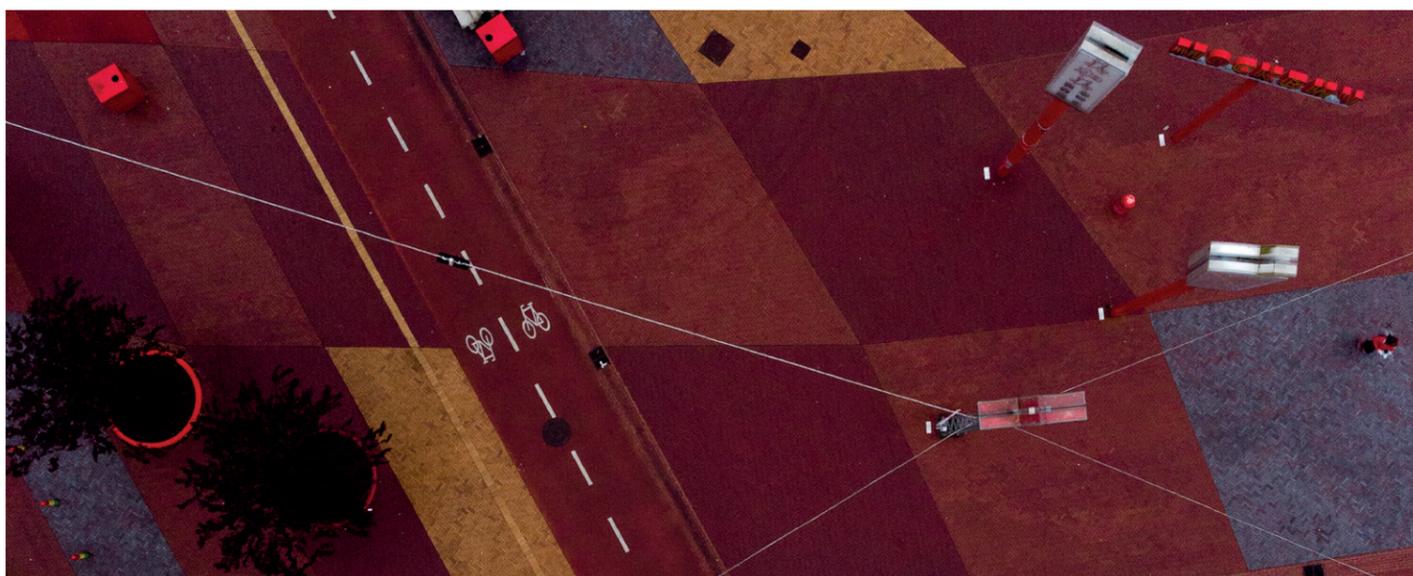
## George Street (St. Andrew Street to Loch Street)



Carrageways can be narrowed to allow for tree planting

George Street has the potential to become an instantly recognisable streetscape, from Spring Garden to the Bon Accord Centre. The role of colour, street planting, street furniture, lighting and signage can create a continuous and animated journey through the neighbourhood.

The George Street corridor should read as the set piece streetscape through the neighbourhood, with consistent elements and streetscape features. However there are also opportunities to introduce local features and spaces along sections of George Street, depending upon the levels of activity and uses that will need to make best use of the transformed streetscape.



Weaving colour and patterns into the streetscape can create points of interest throughout the neighbourhood



## George Street Projects (St. Andrew Street to Loch Street)

The following projects have been identified for George Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors onto George St - painted, restored, highlighted through public realm materials
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Street & pedestrian crossing colouring	Focused on junction with St. Andrew St to connect with pedestrianised section of George Street as well as John St Junction. Colouring could also be focused around the entrance points to the Bon Accord Centre and Norco House
5	Improvement to quality and consistency of shop front signage	Working with local businesses to enhance and co-ordinate shop front signage to create degree of consistency whilst still allowing for individual branding.
6	Bin storage	On-street bin storage is required on-street, provide shelters for screening
7	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
8	Overhead lighting	Located along George Street from St. Andrew Street to Bon Accord Centre, running across the street to illuminate
9	Front door lighting for residential tenements	Working with local residents to enhance their front doors onto George St
10	Uplighting and feature lighting to illuminate our special buildings	Norco House (both external uplighting and internal ground floor window lighting) as well as on listed buildings
11	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review. Create space for pop up events / markets
12	Signage and wayfinding strategy	Co-ordinated as part of wider neighbourhood signage palette and style and should be located around Bon Accord Centre entrance / exit as well as around St. Andrew Street junction. Information totems provided informing of upcoming events within the neighbourhood.
13	Install rain gardens, SuDS and planting which filters out pollutants	Raised planters & rain gardens define service access route and provide attractive integrated seating options.
14	Street tree planting	Variety of street tree types which add colour and definition to George Street. Street trees will add character and softness to the hard landscape proposals.
15	Public spaces / residential grow spaces / play streets	Introduction of adventure and informal play features as part of the public realm. Play spaces should be well defined and separated from service access route.
16	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway</li> <li>Junction radii tightening at junction with St. Andrew St</li> <li>Crossing build-outs at St. Andrew St junction</li> <li>Visual demarcation of service access route with small paving block setts</li> </ul>
17	Street furniture and bin storage	New street furniture and bins.

Project Ref:	Project	Description
<b>George Street as a Destination</b>		
18	Visitor and residential information points	These could be pop up kiosks / interactive stands / live cycle counters / air quality readings etc
19	Provision of basic services.	Explore opportunity to introduce public toilets within Lower George Street
20	Improved CCTV coverage	At suitable locations e.g north of Bon Accord Centre and along Loch Street to Berry and Harriet Street
21	Encourage meanwhile uses such as pop ups, street closures, parklets etc to improve activity	Space identified for pop up markets / events / performances. Could be linked to future use of Norco House or events within Bon Accord Centre.
22	Improved street lighting	Using a variety of lighting options to illuminate the street, Overhead lighting, building up lighters, under seating lighting and wall mounted / street column lighting combined.
<b>Reducing Vehicle Dominance</b>		
23	More flexible use of on-street parking spaces - creating PARKlets which reclaim parking bays for socialising, play and dwell time. Create a PARK(ing) Day which could bring a festival of parklets to the area.	Spaces to be identified with local businesses. Interventions could be permanent or temporary depending upon approach. Opportunity to link with future community events / festivals etc
24	Quality of cycle parking and infrastructure	Cycle parking provision should be provided around Bon Accord Centre and other significant uses

**Table 4 - George Street Projects**

## George Street (John Street to St. Andrew Street)

This section of George Street is to be transformed into a vibrant and bustling street that is redesigned to provide more space for people. It will visually and functionally connect with the southernmost section of George Street, redefining the function of the street, creating a new linear destination for visitors and the community.

Identified as a primary street and 'community connector' in the Masterplan, it will be reconfigured to expand the space available for pedestrians, active travel and for spill out space for local businesses. Vehicle access will be restricted to, time limited, service access only. There are opportunities to introduce new street trees, planting beds, comfortable dwell spaces, lighting and signage, all of which subtly combines to create a comfortable and distinctive destination. Service vehicle traffic is guided along the street through traffic calming and street furniture measures.

Urban parklets, pop-up commercial spaces and spaces to pause will redefine this section of George Street, encouraging discovery but also allowing time to appreciate the historic character and the vibrancy of local businesses.

George Street (John Street to Andrew Street)		
Primary Street		
	Current Design	Proposed Illustrative Design
<b>Street Design</b>		
Typical carriageway width	9.86m	Decreased to c8.6m
Typical road width	6.37m	Decreased to c3m
Footpath width	2.55m / 2.17m	Increased 5.8m either side of access road
On Street parking	Y - circa 11 spaces	N
Loading bays	Y	Y
Bus route and stops	Y	N
<b>Materiality</b>		
Carriageway	Hot rolled asphalt	Granite tonal setts
Footway	Hot rolled asphalt	Dark grey Whinstone slabs
<b>Streetscape</b>		
Refuse storage	Y	Y
Street planting	N	Y - parklets, rain gardens & tree planting
Street furniture	N	Y - New street lighting, seating, bins, signage, cycle parking



Figure 28 - Existing street section

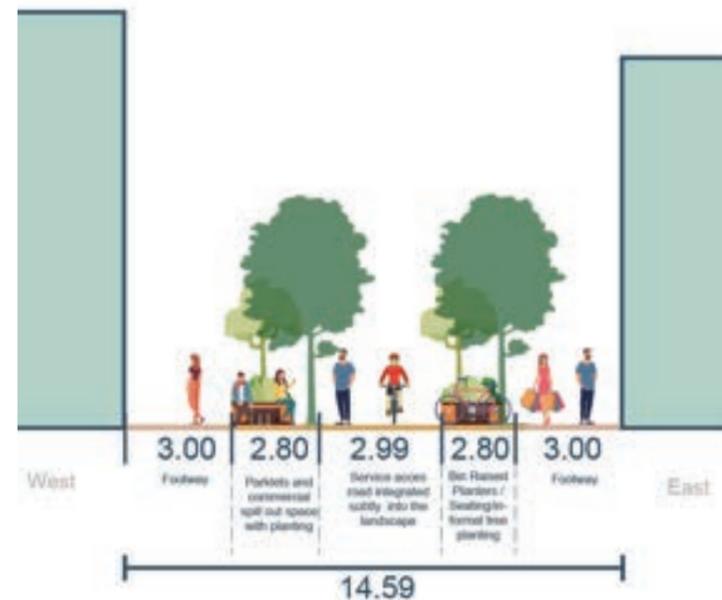


Figure 29 - Proposed street section

Table 5 - George Street Details



Figure 31 - Concept plan view along George Street

Figure 30 - Location within the Masterplan

## George Street Projects (John Street to St. Andrew Street)

The following projects have been identified for George Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors onto George St - painted, restored, highlighted through public realm materials
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Street & pedestrian crossing colouring	Included along full length of this section of George Street as well as at both John St and St. Andrew Street junctions. Colouring to be determined but could be bold bright and distinctive to George Street. Creating a visual impact that create interest and distinction - instagramable
5	Improvement to quality and consistency of shop front signage	Working with local businesses to enhance and co-ordinate shop front signage to create degree of consistency
6	Bin storage	Where on-street bin storage is required on-street, provide shelters for screening. Could include green roofs
7	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
8	Festoon lighting / lighting net	Introduce new feature lighting along this section of George Street which could include a light net, festoon / centenary lighting as well as distinct up-stand lighters as part of the street furniture palette
9	Front door lighting for residential tenements	Working with local residents to enhance their front doors onto George St
10	Up-lighting and feature lighting to illuminate our special buildings	See lighting strategy - lighting interventions (up-lighters) to key historic / culturally significant buildings
11	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review
12	Signage and wayfinding strategy	Co-ordinated signage palette and types along George St . Design to be determined but could include current and historic information about the area as well as wayfinding.
13	Install rain gardens, SuDS and planting which filters out pollutants	Should look to introduce SuDS as far as possible to soften urban environment, bring colour and well-being opportunities
14	Introduction of street tree planting	New tree planting should be included in this section of George St, as per the tree hierarchy described earlier in this section
15	Public spaces	Introduction of play street features within the street section - temporary and permanent. Trim trails and puzzle play could work well here
16	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway to service access only</li> <li>Footway widening on both sides of street</li> <li>Junction radii tightening at junction with John Street &amp; St. Andrew Street</li> <li>Crossing build outs at all junctions</li> <li>New palette of materials along all of George Street</li> <li>Uniform materiality along street with insets to depict service access</li> <li>Supergraphics to be included</li> </ul>
17	Street furniture and bin storage	Introduce bold, colourful and contemporary new street furniture as part of the visual identity of the place. Should encourage individual and groups to sit, relax and pause comfortably.



An example image of a possible way that restricting private vehicle movement through George Street will prioritise space for pedestrians

Table 6 - George Street Projects



Figure 32 - Illustrative concept view of George Street

# Harriet Street

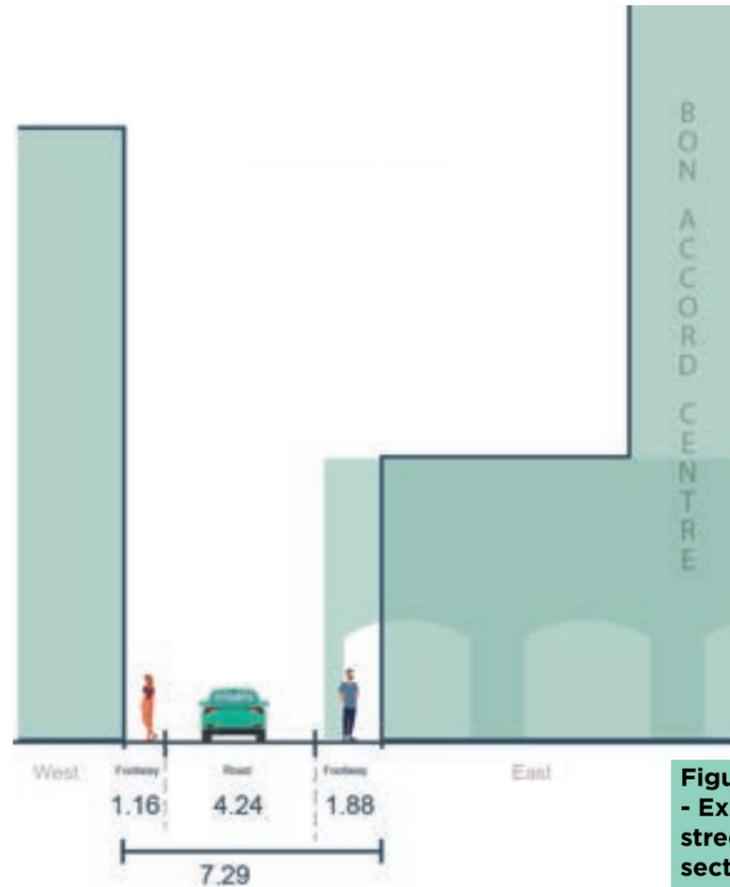
Harriet Street is the main 24/7 connection from the heart of the city centre to George Street and experiences high levels of traffic through movements in a north bound direction. It is also a well used pedestrian route.

The street retains its northbound vehicle movement, with a reduced road width. This allows for more space to be given over to pedestrian movement along the eastern side of the carriageway and for more comfortable and safer pedestrian journeys. The junction with Schoolhill is historically narrow, creating a constrained junction for active travel. The proposed street section assumes cycle access along the carriageway as part of a 'mixed street'

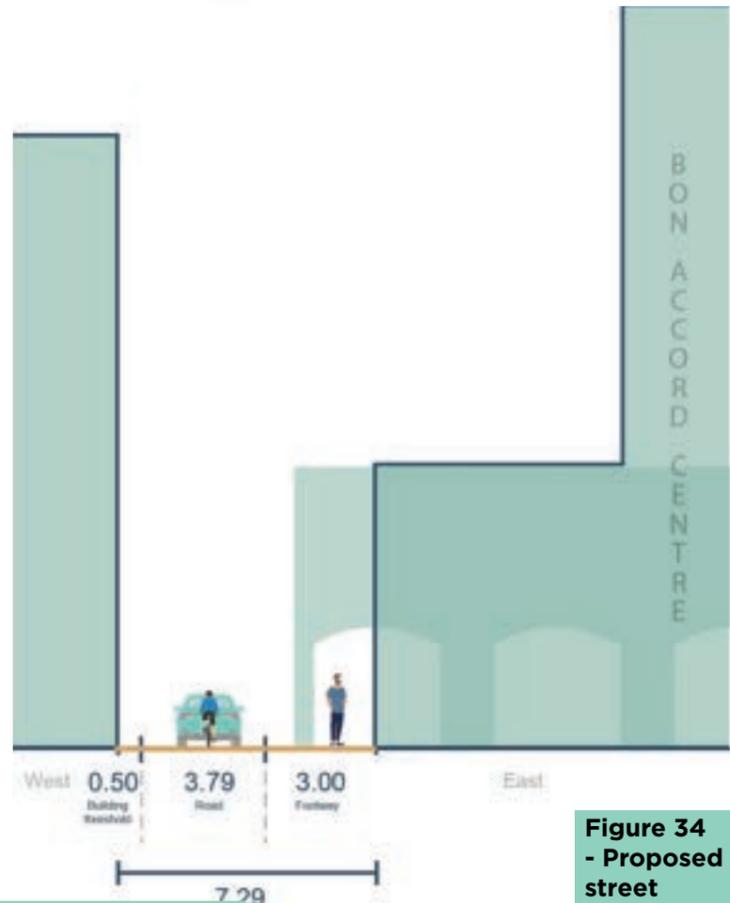
The introduction of a lighting net will illuminate the street in the evenings and create a striking route through to the George Street area. Other interventions such as vertical greening along dead frontages, murals / artworks and better signage and wayfinding markers at the entrances to the street and along it will help people more easily navigate through towards George Street or the city centre.

Harriet Street		
Tertiary Street		
	Current Design	Proposed Illustrative Design
<b>Street Design</b>		
Typical carriageway width	4.24m	Decreased to c3.80m
Typical road width	4.24m	Decreased to c3.80m
Footpath width	1.16m / 1.66m	Resized to 3m on eastern side
On Street parking	N	N
Loading bays	N	N
Bus route and stops	N	N
Designated cycle route	N	N - cycle friendly street
<b>Materiality</b>		
Carriageway	Hot rolled asphalt	Granite tonal setts
Footway	Hot rolled asphalt	Dark grey Whinstone slabs
<b>Streetscape</b>		
Refuse storage	N	N
Street planting	N	Y - localised vertical greening
Street furniture	N	Y - Improved street lighting

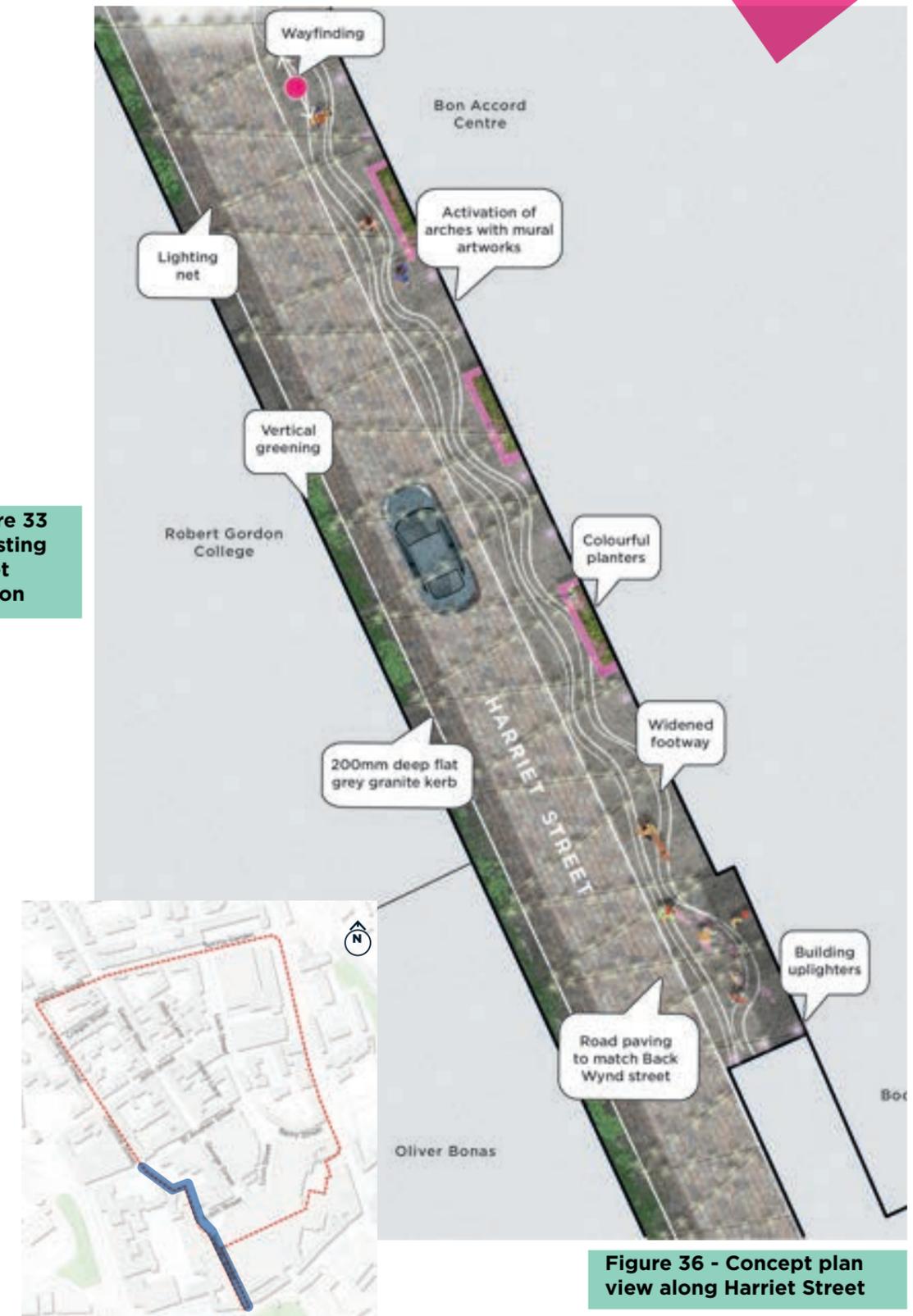
**Table 7 - Harriet Street Details**



**Figure 33 - Existing street section**



**Figure 34 - Proposed street section**



**Figure 36 - Concept plan view along Harriet Street**

**Figure 35 - Location within the Masterplan**

## Harriet Street Projects

The following projects have been identified for Harriet Street which will contribute to securing the 'Place' objectives of the Masterplan.

Project Ref:	Project	Description
<b>Appearance and Character</b>		
1	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
2	Street & pedestrian crossing colouring	Bold colouring limited to walls, however material changes at crossings will include tonal changes. Materiality should reflect that at Back Wynd.
3	Visual screening across poor quality environments	Introduce green walls, artist installations, colour and lighting to hide or screen any visually unsightly parts of the street
<b>Enhanced Place Experience</b>		
4	Festoon lighting / lighting net	Localised to length between Schoolhill and Crooked Lane
5	Uplighting and feature lighting to illuminate our special buildings	Uplighting where appropriate along Bon Accord Centre.
6	Signage and wayfinding strategy	Co-ordinated signage palette and types along Harriet Street. Works in combination with festoon signage already in place.
7	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway</li> <li>Footway widening on eastern side of street</li> <li>New palette of materials</li> </ul>
8	Improved provision of street furniture and bin storage	Introduce seating along Harriet Street to provide opportunities for rest and relaxation
<b>George Street as a Destination</b>		
9	Improved CCTV coverage	At suitable locations e.g Schoolhill junction and Loch Street junction.

**Table 8 - Harriet Street Projects**



Playful lighting schemes can brighten narrow lane ways



**Figure 37 - Illustrative concept view along Harriet Street**

## George Street (Spring Garden to John Street)

This northernmost section of George Street within the Masterplan will continue to function as the neighbourhood's vibrant and diverse local 'high' street, becoming significantly enhanced, creating a more sustainable, inviting and people focused place that better manages traffic movement along it. Carriageway reconfiguration will provide more pedestrian and spill out space, will formalise parking spaces and will both physically and visually narrow the carriageway. Additions of colour and signage will help to create a welcoming and distinctive arrival to the neighbourhood from the north.

General private vehicle traffic along this primary, community connector street is allowed to travel southbound only, with bus movements allowed in both directions.

George Street (Spring Garden to John Street)		
Primary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	10.15m	Decreased to 9.91m
Typical road width	7.15m	Increased to 6.2m
Footpath width	2.37m / 2.19m	Increased to 2.4m
On Street parking	Y - circa 15 spaces	Y
Loading bays	Y	Y
Bus route and stops	Y	Y
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Hot rolled asphalt	Dark grey Whinstone slabs
Streetscape		
Refuse storage	Y	Y
Street planting	N	Y - parklets, rain gardens & tree planting
Street furniture	N	Y - New street lighting, seating, bins, signage, cycle parking

**Table 9 - George Street Details**



**Figure 38 - Existing street section**



**Figure 39 - Proposed street section**



**Figure 41 - Concept plan view along George Street**



**Figure 40 - Location within the Masterplan**

# George Street Projects (Spring Garden to John Street)

The following projects have been identified for George Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors onto George St - painted, restored, highlighted through public realm materials
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Street & pedestrian crossing colouring	Focused on junction with John Street to connect with redesigned middle part of George Street as well as at Spring Garden junction
5	Improvement to quality and consistency of shop front signage	Working with local businesses to enhance and co-ordinate shop front signage to create degree of consistency
6	Bin storage	On-street bin storage is required on-street, provide shelters for screening. Could include green roofs
7	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
8	Festoon lighting / lighting net	Introduce new feature lighting along this section of George Street which could include a light net, festoon / centenary lighting as well as distinct up-stand lighters as part of the street furniture palette
9	Front door lighting for residential tenements	Working with local residents to enhance their front doors onto George St
10	Up-lighting and feature lighting to illuminate our special buildings	See Lighting Strategy - lighting interventions (up-lighters) to key historic / culturally significant buildings
11	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review
12	Signage and wayfinding strategy	Co-ordinated signage palette and types along George St foot-ways. Signage design should be consistent with that along the southern sections of George Street towards the City Centre
13	Install rain gardens, SuDS and planting which filters out pollutants	Should look to introduce SuDS as far as possible to soften urban environment, bring colour and well-being opportunities
14	Street tree planting	New tree planting should be included in this section of George St, as per the tree hierarchy described earlier in this section
15	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway</li> <li>Footway widening on both sides of street</li> <li>Junction radii tightening at junction with John Street, Spring Garden &amp; Craigie Street</li> <li>Crossing build-outs at all junctions</li> <li>Formalised parking bays on east side of street</li> <li>New palette of materials</li> <li>Potential to include temporary parklet spaces within parking areas for events to allow for more activity and dwell time</li> </ul>
16	Street furniture and bin storage	Introduce bold, colourful and contemporary new street furniture as part of the visual identity of the place. Bin storage to be integrated through screening / improve storage

**Table 10 - George Street Projects**



Pops of colour within the carriageway can create a playful streetscape



Rain gardens and planting help separate movement flows



Bin storage can be integrated into the street to look less untidy

## Craigie Street

Craigie Street is a tertiary street that has a considerably different aesthetic and function to the primary routes through the area. It is a low flow, low traffic street which presents opportunities to introduce interventions that benefit residents along it.

The key focus of transformation on this street is to slow traffic passing through, to add colour and soft landscape features to the streetscape and enhancing the environment for local residents through dwell / grow and play spaces. This change in character and better management of through traffic and on-street parking will create a people focused street for local residents..

The introduction of planting will visually narrow the carriageway and define seating areas and dwell space for local residents.

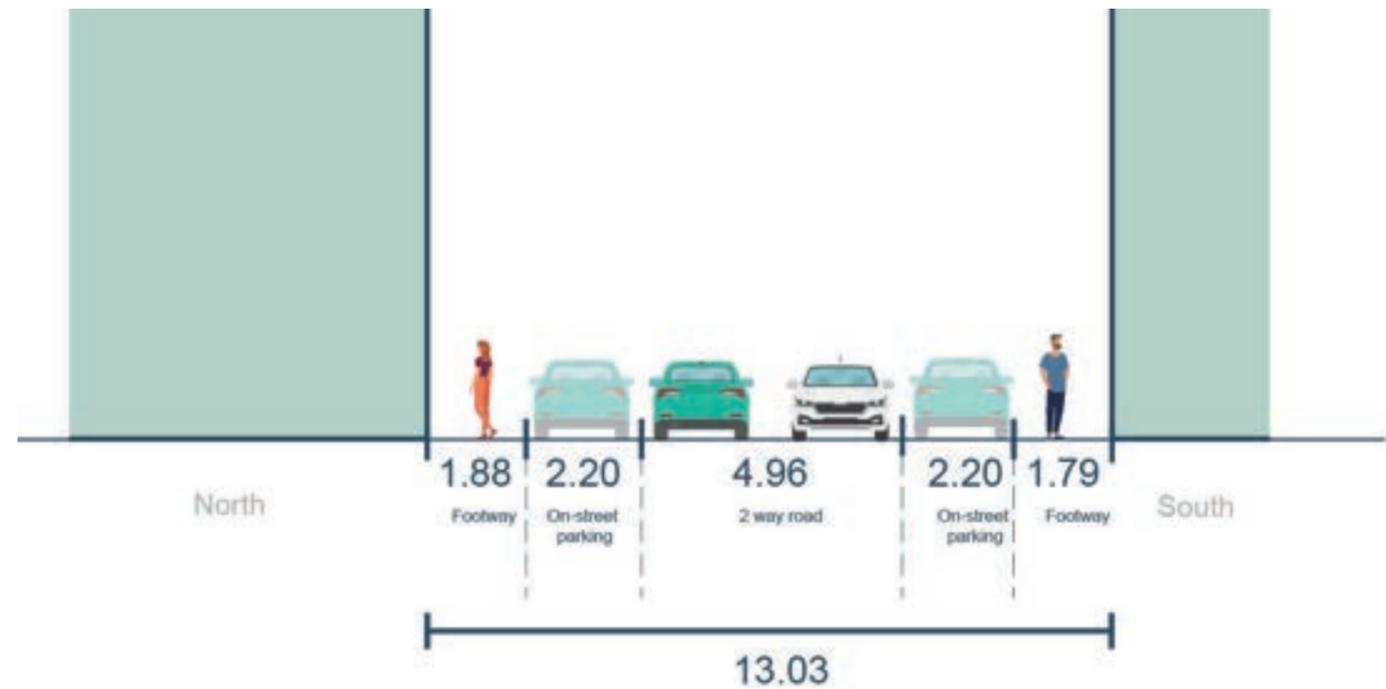


Figure 42 - Existing street section

Craigie Street		
Tertiary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical Carriageway width	c9.4m	c9.4m
Typical Road width	c5.0m	c5.0m
Footpath width	c1.9m / c1.8m	c1.9m / c1.8m
On street parking	Yes circa 22 bays	Formalised parking bays with footway build outs.
Loading bays	N	N
Bus route and stops	N	N
Materiality		
Carriageway	Granite setts	Granite setts
Footway	Paving slabs	Black Granite Slab / Dark grey Whinstone Slab
Streetscape		
Refuse storage	Y	Y
Street planting	N	Y - intermittently on both sides of carriageway
Street furniture	N	Y - raised planters & seating

Table 11 - Craigie Street Details

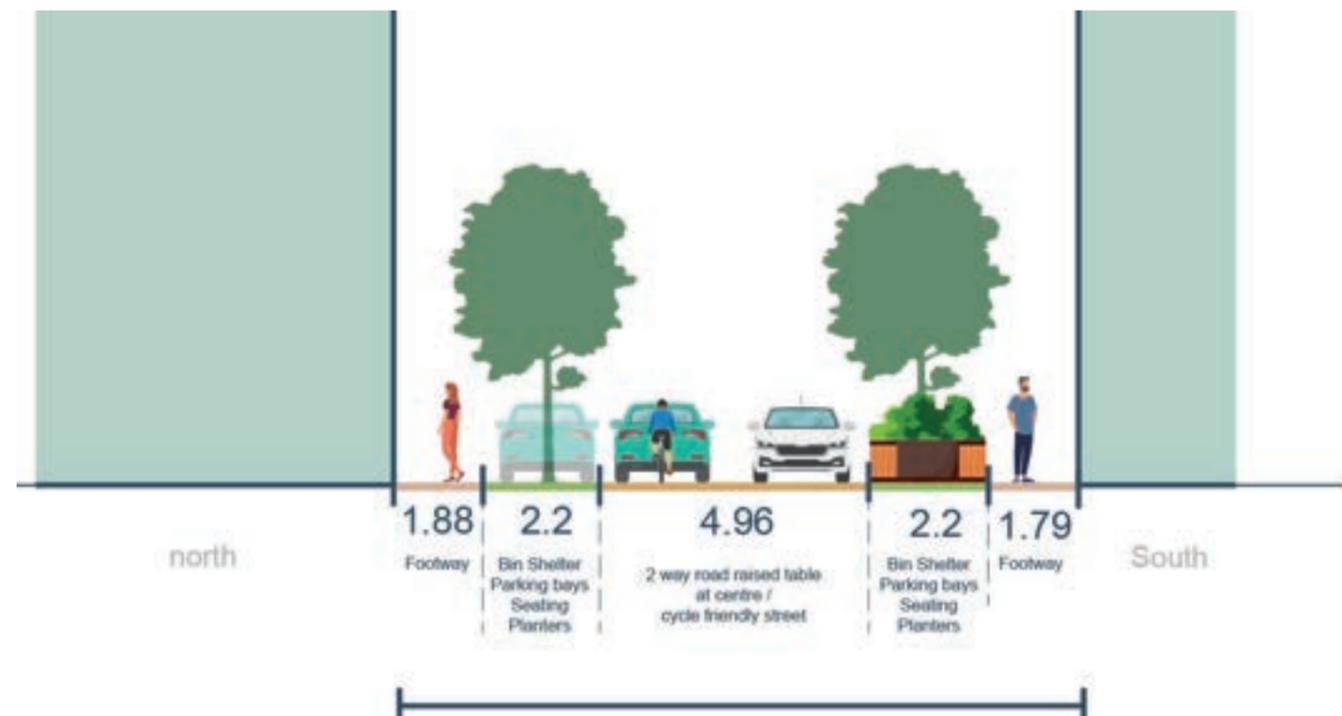


Figure 43 - Proposed street section

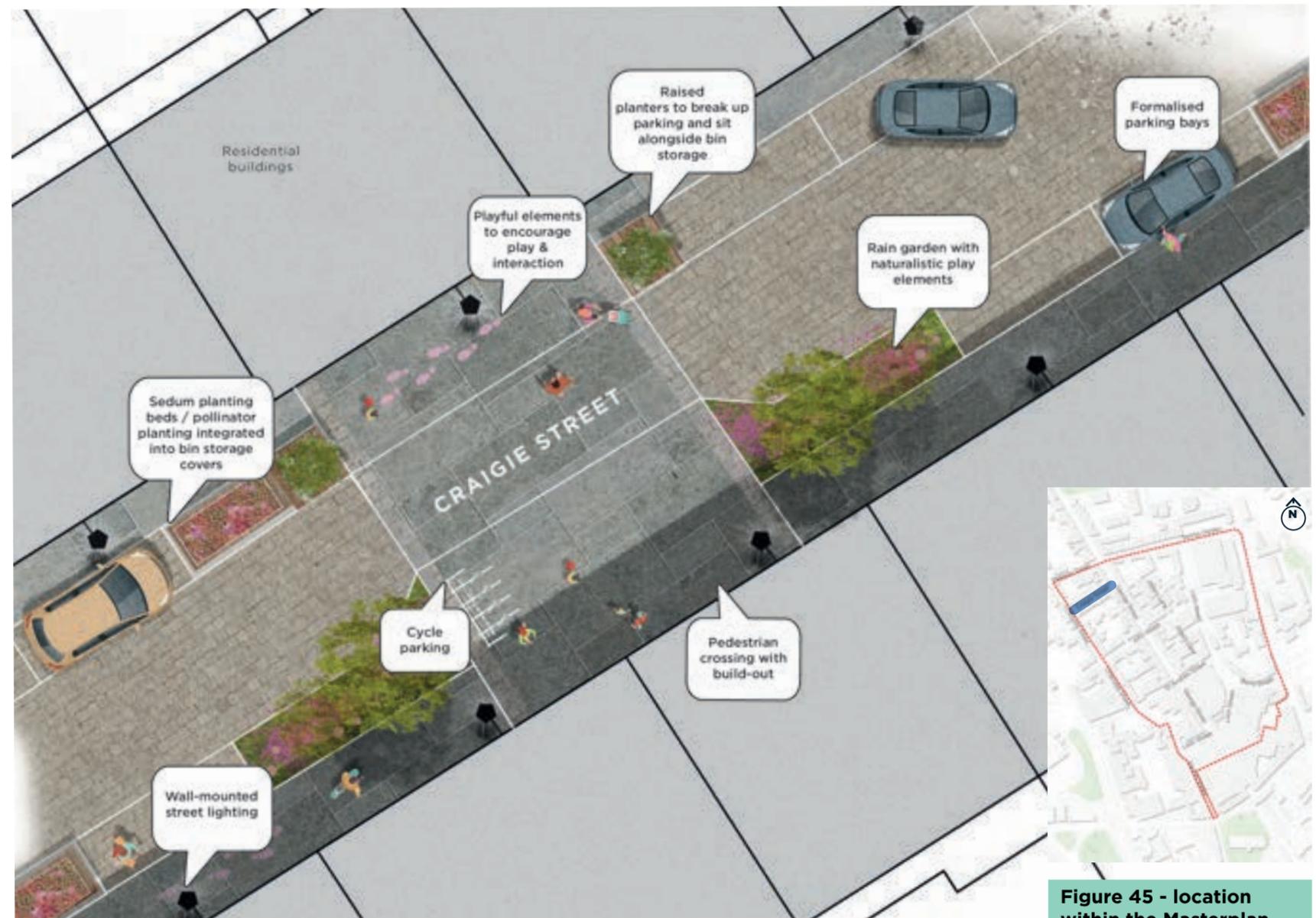
# Craigie Street Projects

The following projects have been identified for Craigie Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors onto Craigie Street - painted, restored, highlighted through public realm materials & lighting
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Bin storage	Where on-street bin storage is required on-street, provide shelters or visual screening
<b>Enhanced Place Experience</b>		
5	Front door lighting for residential tenements	Working with local residents to provide wall mounted front door lighting to improve safety
6	Signage and wayfinding strategy	Co-ordinated welcome signage palette and types, located at the entrances to Craigie Street
7	Install rain gardens, SuDS and planting which filters out pollutants.	Raised planters and build outs with sensory and pollinator friendly planting / food production within the carriageway
8	Introduction of street tree planting	Flowering street trees or fruit trees provided within the carriageway to help define parking spaces / dwell areas
9	Provision of public spaces / residential grow spaces / play streets	Introduction of play features to encourage local community interaction and active lifestyles
10	Carriageway redesign	<ul style="list-style-type: none"> <li>Visual narrowing of vehicle carriageway</li> <li>Footway widening on both sides of street</li> <li>Junction radii tightening at junction with George Street &amp; Craigie Street</li> <li>Planter build outs slow traffic movement</li> <li>Formalised parking bays on both sides of street</li> </ul>
<b>George Street As A Destination</b>		
11	Improved CCTV coverage	At suitable locations e.g junction with George Street
<b>Reducing Vehicle Dominance</b>		
12	More flexible use of on-street parking spaces - creating PARKlets which reclaim parking bays for socialising, play and dwell time.	Spaces to be identified with local residents. Interventions could be permanent or temporary depending upon approach. Opportunity to link with future community events / festivals etc across the area.

**Table 12 - Craigie Street Projects**

**Figure 44 - Concept plan of potential projects along Craigie Street**



**Figure 45 - location within the Masterplan**

## Loch Street

Loch Street is a wide street connecting Spring Garden to Berry Street in a north south direction. Its width, although not consistent along it, allows for sufficient on street parking; however it is notably dominated by vehicle movement and has limited street planting and furniture which diffuses any positive sense of place or character.

The currently wide street section will be reconfigured to provide improved active travel links to and from NESCOL. At its northernmost end, the carriageway can accommodate segregated cycle lanes in northbound and south bound directions until the street section narrows at the junction with John Street. The segregated cycle lanes continue onto the eastern end of John Street and onto the improved NCN Route along Gallowgate.

Immediately adjacent to NESCOL, there is an opportunity to introduce break out spaces, cycle storage and tree planting, providing a more user friendly space for visitors and students to the College.

Interventions in this section of the street mean a loss of on-street car parking but this could be accommodated elsewhere within the street network or could be explored further south along Loch Street. It should be noted that further south, the existing parking bays would be retained and improved.

Loch Street		
Secondary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	9.30m	Increased to 9.60m (to include cycle lane provision)
Typical road width	7.10m	Decreased to 5.60m
Footpath width	2.40m / 3.50m	Decreased to 1.80m / 2.00m
On Street parking	Y - circa 17 spaces + 2 accessible spaces	Y - In part where street width allows. Accessible spaces retained
Loading bays	N	N
Bus route and stops	N	N
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Hot rolled asphalt	Dark grey Whinstone slabs
Streetscape		
Refuse storage	Y	Y
Street planting	N	Y - adjacent to college as part of breakout space
Street furniture	N	Y - New street lighting, seating, bins, signage, cycle parking

Table 13 - Loch Street Details

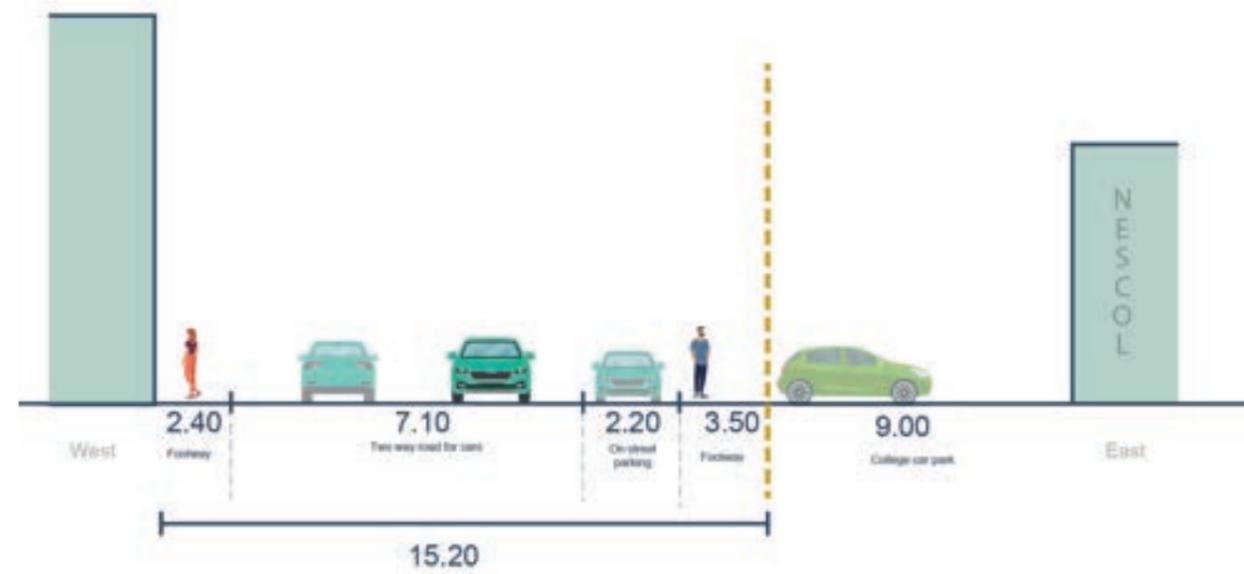


Figure 46 - Existing street section

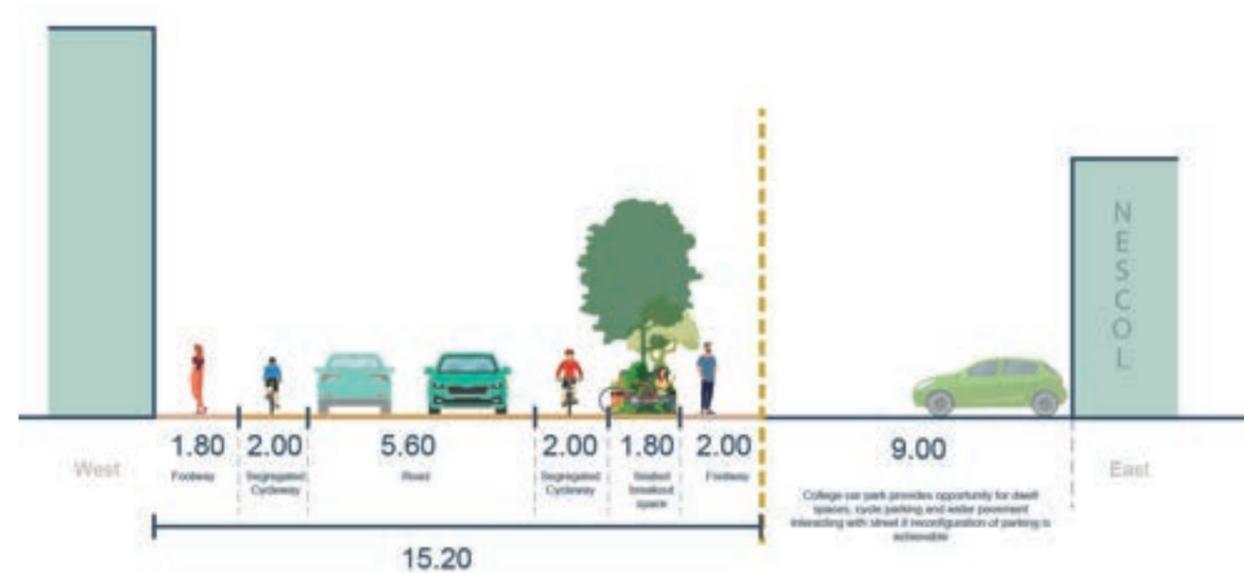


Figure 47 - Proposed street section

## Loch Street Projects

The following projects have been identified for Loch Street which will contribute to securing the Place Objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant street signage and temporary signage to reveal built character
2	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
3	Street & pedestrian crossing colouring	Focused on junction with John Street to connect with redesigned middle part of George Street as well as at Spring Garden junction
4	Visual screening across poor quality environments	Boundary of NESCOL to be activated with seating and planters, visually connecting NESCOL into George Street
5	Bin storage	Where commercial on-street bin storage is required on-street, provide shelters for screening. Could include green roofs
6	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
7	Enhanced street lighting	Introduce additional lighting along Loch Street, particularly at NESCOL, which could include distinct vehicular column lighting and wall mounted lighting as part of the street furniture palette
8	Up-lighting and feature lighting to illuminate our special buildings	See lighting strategy - lighting interventions (vehicular column lighting and wall mounted lighting).
9	Create active uses and frontage	Temporary uses within vacant buildings - specific buildings to be identified / kept under review
10	Signage and wayfinding strategy	Co-ordinated signage palette and types along Loch St foot-ways. Signage design should be consistent with that throughout the neighbourhood towards the City Centre
11	Introduction of street tree planting	New tree planting should be included along Loch St, to enhance the streetscape and visually narrow the carriageway
12	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway</li> <li>Pavement build-outs where possible</li> <li>Retention of on street parking where possible</li> <li>New palette of materials</li> <li>2m cycle lane either side of carriageway</li> <li>Cycle parking outside NESCOL</li> </ul>
13	Provision of street furniture and bin storage	Introduce bold, colourful and contemporary new street furniture as part of the visual identity of the place. Bin storage to be integrated through screening / improve storage

**Table 14 - Loch Street Projects**



**Figure 48 - Location within the Masterplan**

**Figure 49 - Concept plan view along Loch Street**

## Berry Street

A key part of the street network, Berry Street will continue to perform a number of functional roles as well as providing an enhanced new visual and active travel connection into George Street from Gallowgate. This section of Berry Street provides an open, public link into the Masterplan area and onwards towards Loch Street, George Street and St. Andrew Street. It can become a positive, welcoming street that invites pedestrian and active travel through movement but limits vehicular movements.

The existing street section is wide and infrastructure dominated. There is an opportunity to reconfigure the street width and current roundabout to an arrangement that frees up land that could be a new pocket park, linked to 'Norco Place' (**Transformational Project 1**). Tightening of the street carriageways would allow for more tree planting, wider footway provision, resident and visitor seating and more SUDs and wildflower planting accommodated.

Access to the Bon Accord Shopping Centre MSCP is required and this would still be achieved via access along a suitable access route for entry which also provides service access onto Loch Street. Egress from the MSCP would be via Berry Street, with the central median removed to allow right turns. The final design approach is to be determined, but the illustrative plan below sets out a potential response to the opportunity along Berry Street. Bus, taxi and private hire movements would continue in both directions along Berry Street onto Gallowgate.

Berry Street		
Secondary Street		
	Current Design	Proposed Illustrative Design
<b>Street Design</b>		
Typical carriageway width	12.09m (including central reservation)	Increased to 12.44m (to include rain gardens and cycle lane)
Typical road width	5.77m / 4.72m	Increased to 6.5m
Footpath width	2.28m / 6.07m	Decreased to 2m / /Decreased to 4.50m
On Street parking	N	N
Loading bays	Y	Y
Bus route and stops	Y	Y - east bound onto Gallowgate
Designated cycle route	N	Y - 2 way from Gallowgate to Loch Street
<b>Materiality</b>		
Carriageway	Hot rolled asphalt	Granite tonal setts
Footway	Hot rolled asphalt	Dark grey Whinstone slabs
<b>Streetscape</b>		
Refuse storage	Y	Y
Street planting	Y	Y - Retain existing trees and introduce new street planting
Street furniture	N	Y - New street lighting, informal and formal seating, bins, gateway signage onto Gallowgate, cycle parking

Table 15 - Berry Street Details



Figure 50 - Existing street section



Figure 51 - Proposed street section



**Figure 53 - Concept plan view along Berry Street**



**Figure 52 - Location within the Masterplan**

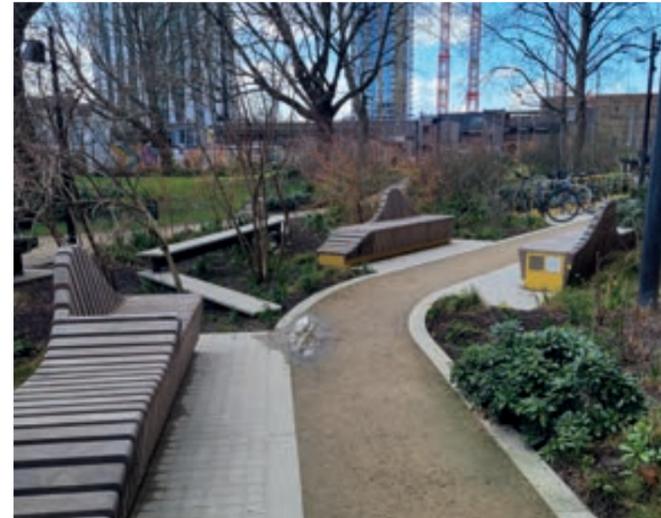
The above concept plan illustrates how new public space could be created through the rationalisation of street infrastructure and reducing the dominance of vehicle movements through this area. It assumes that future proposals for Norco House propose removal of the 1980s extension, which will allow for further expansion of this space, as illustrated in **Transformational Project 1**.

## Berry Street Projects

The following projects have been identified for Berry Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant street signage and temporary signage to reveal built character
2	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
3	Street & pedestrian crossing colouring	Included at the junction of Loch Street, St. Andrew Street and Berry Street, and including area of new public realm surrounding Norco House. Colouring to be determined but could be bold bright and distinctive to the neighbourhood. Creating a visual impact that creates interest and distinction - instagramable
4	Bin storage	Public use bins will be provided within the public realm and pocket park
5	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
6	Up-lighting and feature lighting to illuminate our special buildings	Introduce bespoke lighting within the public realm at Berry Street Norco House which could include distinct up-stand lighters as part of the street furniture palette. See lighting strategy - lighting interventions (up-lighters) to key historic / culturally significant buildings
7	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review. Vertical greening could also be applied to screen car parks and blank façades.
8	Signage and wayfinding strategy	Co-ordinated signage palette and types along Berry Street, connecting back to George Street and the City Centre. Design to be determined but could include current and historic information about the area as well as wayfinding.
9	Install rain gardens, SuDS and planting which filters out pollutants.	Should look to introduce SuDS as far as possible to soften urban environment, bring colour and well-being opportunities
10	Introduction of street tree planting	New tree planting should be included in Berry Street.
11	Public spaces	Introduction of furniture and amenities which allow people to dwell in the area and interact with its offer.
12	Carriageway redesign	<ul style="list-style-type: none"> <li>Inclusion of 3m 2-way cycle lane from Gallowgate to Loch Street</li> <li>Car park exit to be right turn only towards Gallowgate</li> <li>Junction radii tightening at Berry Street to Gallowgate</li> <li>New palette of materials along all of Berry Street to Loch street</li> <li>Uniform materiality along street with insets to depict service access</li> <li>Supergraphics to be included</li> <li>Rain gardens and street planting included</li> </ul>
13	Street furniture and bin storage	Introduce bold, colourful and contemporary new street furniture as part of the visual identity of the place. Should encourage individual and groups to sit, relax and pause comfortably.

**Table 16 - Berry Street Projects**



Seating included within public spaces will encourage people to sit and dwell



Rain gardens and planting will create and frame dwell spaces



Bringing colour from the street onto buildings can shape the space and create a visual identity

## St. Andrew Street (George Street to Loch Street)

St. Andrew Street is a central street within the Masterplan and is an important primary street connection that provides links from Loch Street through to Blackfriars Street.

This section of St. Andrew Street will provide two way bus services as well as west bound private car journeys. The carriageway is widened to comfortably allow for 'mixed streets' and cyclist accessibility along them.

Carriageway reconfiguration will provide street tree planting along the southern side of the street along Norco House, creating a tree lined street to compliment the environment surrounding Norco House and the proposed new public space along Loch Street.

Rain gardens, formalised parking bays and material upgrades will help to transform the quality of the streetscape and create opportunities for landscape features. New bus shelters will be set into the carriageway to create more comfortable pedestrian routes.

St. Andrew Street (George Street to Loch Street)		
Primary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	9.23	Increased to c9.8m (to include street planting, bus shelters and parking)
Typical road width	5.53m	Increased to 6.2m
Footpath width	1.59m / 3.97m	Increased to 2m / decreased to 3m
On Street parking	N / 2 accessible spaces	Retain existing number of accessible spaces
Loading bays	N	N
Bus route and stops	Y	Y
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Paving slabs	Dark grey Whinstone slabs
Streetscape		
Refuse storage	Y	Y
Street planting	Y	Y - on both sides of carriageway
Street furniture	Y	Y - within footway provision

Table 17 - St. Andrew Street Details

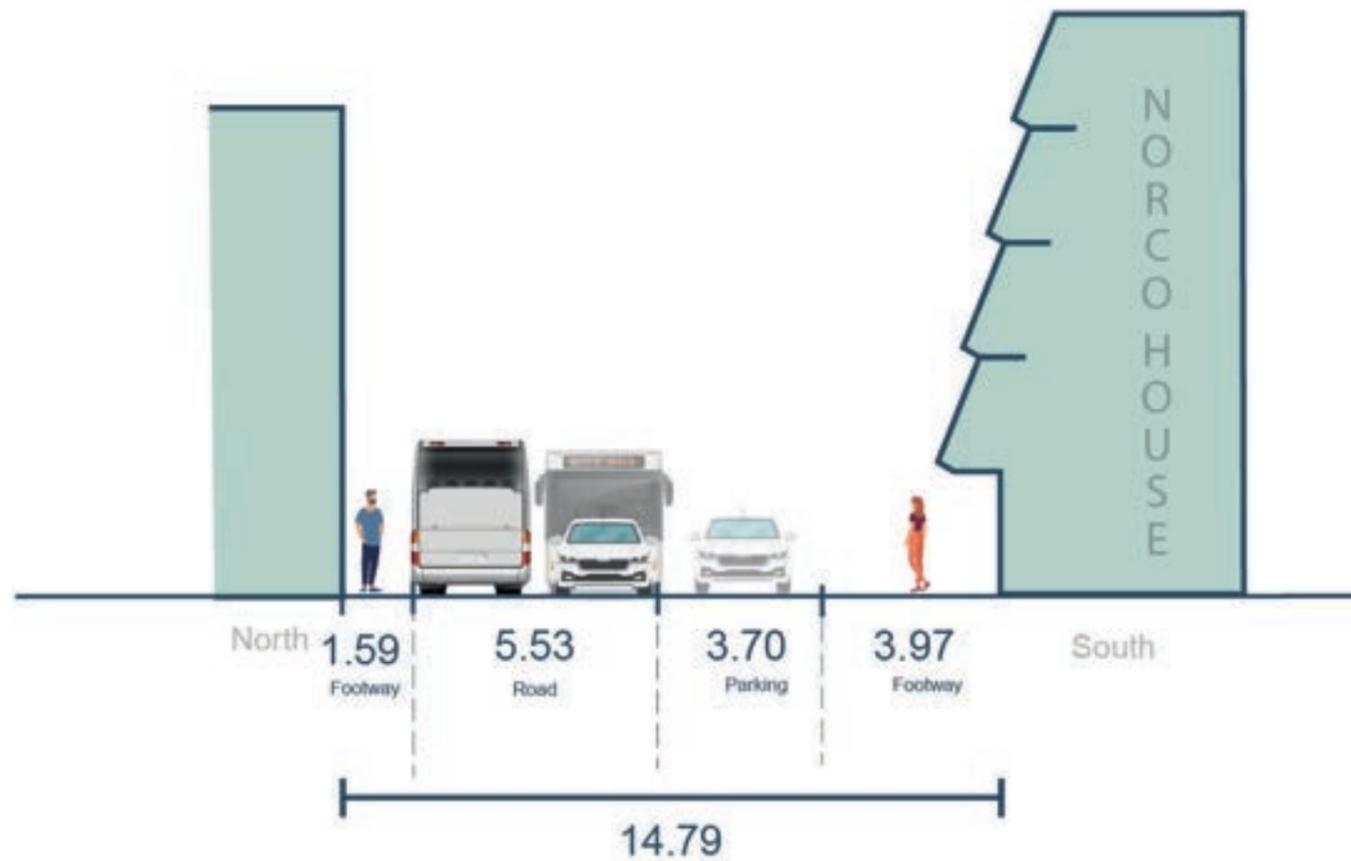


Figure 54 - Existing street section

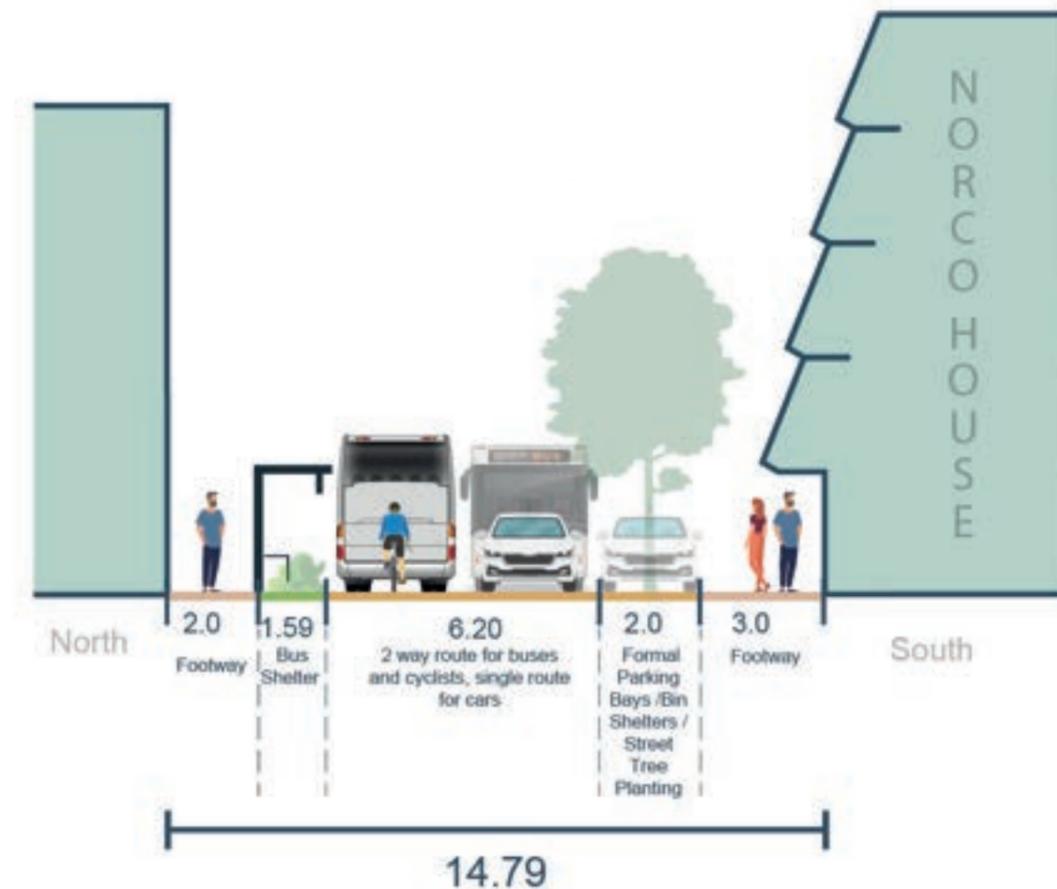


Figure 55 - Proposed street section

## St. Andrew Street (George Street to Blackfriars Street)

The character of the western section of St. Andrew Street changes to a mixed use commercial area of the Masterplan. St. Andrew Street is still an important primary street connection that provides links from Loch Street through to Blackfriars Street.

This section of St. Andrew Street will provide two way bus services as well as west bound private car journeys. The carriageway is widened to comfortably allow for 'mixed streets' and cyclist accessibility along them.

Carriageway reconfiguration will provide street tree planting along the southern side of the street. Combined with pockets of seating and new shrub planting it will create more visual screening to the street and improve the setting for existing business.

Rain gardens provide opportunities to bring more colour through perennial planting and sustainable urban drainage features to better manage surface water run-off. Combined with enhanced materials the streetscape will be significantly improved.

St. Andrew Street (George Street to Blackfriars)		
Primary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	7.67m	Increased to 8.7m (to include rain gardens and street planting)
Typical road width	7.67m	Decreased to 6.2m
Footpath width	2.69m / 2.86m	Decreased to 2m / decreased to 2.5m
On Street parking	N	N
Loading bays	Y	Y
Bus route and stops	Y	Y
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Slabs	Dark grey Whinstone slabs
Streetscape		
Refuse storage	Y	Y
Street planting	Y	Y - on both sides of carriageway
Street furniture	Y	Y - within footway provision

Table 18 - St. Andrew Street Details

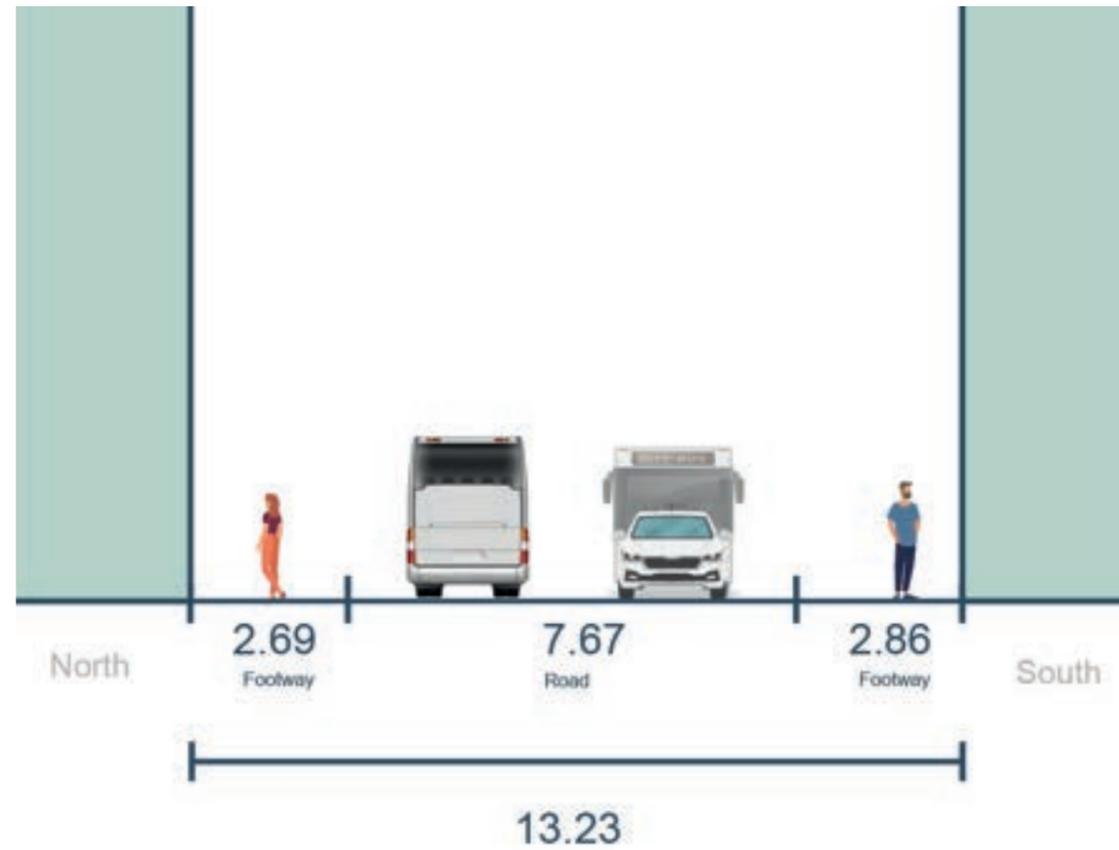


Figure 56 - Existing street section

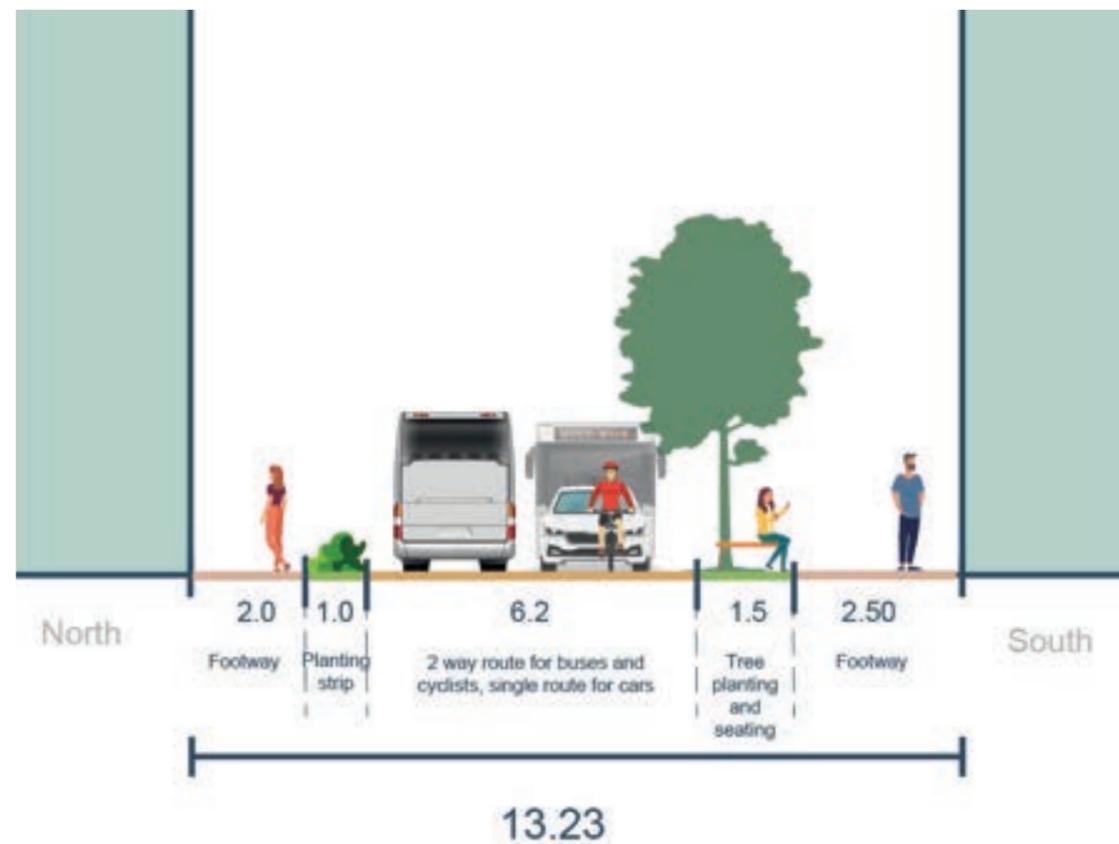
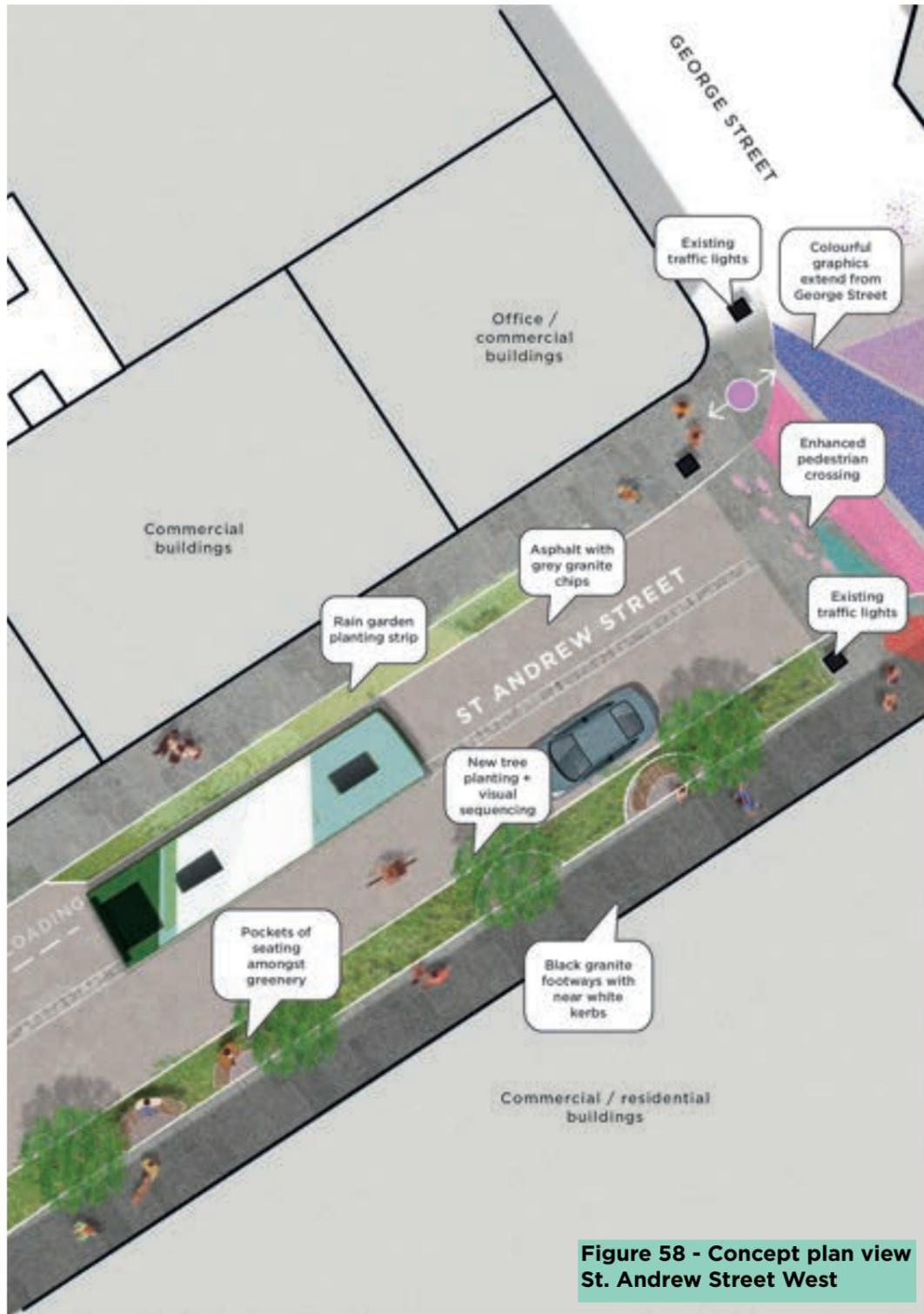


Figure 57 - Proposed street section



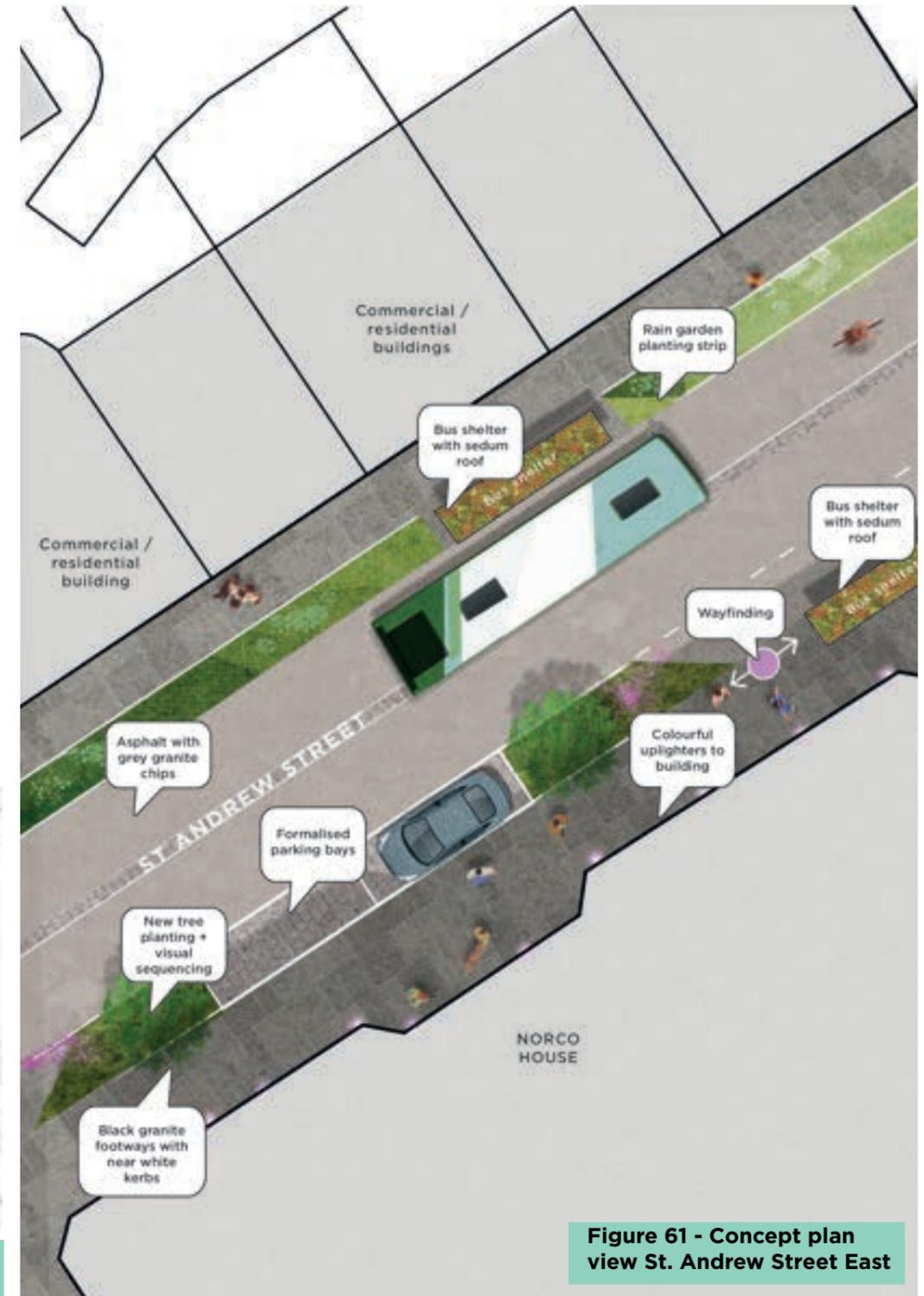
**Figure 58 - Concept plan view St. Andrew Street West**



**Figure 59 - Location within the Masterplan**



**Figure 60 - Location within the Masterplan**



**Figure 61 - Concept plan view St. Andrew Street East**

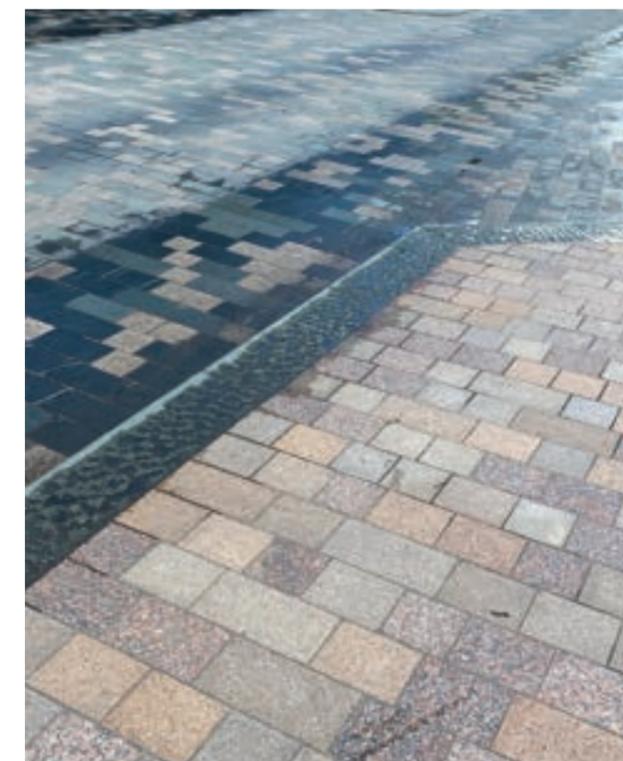
## St. Andrew Street Projects (both east & west of George Street)

The following projects have been identified for St. Andrew Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of weeds, redundant street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors onto George St - painted, restored, highlighted through public realm materials
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Street & pedestrian crossing colouring	Focused on junctions with George Street and Jopp's Lane and Charlotte Street
5	Improvement to quality and consistency of shop front signage	Working with local businesses to enhance and co-ordinate shop front signage to create degree of consistency
6	Bin storage	Where on-street bin storage is required on-street, provide shelters for screening
7	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
8	Uplighting and feature lighting to illuminate our special buildings	Norco House (both external uplighting and internal ground floor window lighting) as well as listed buildings. Also working with local residents to enhance their front doors onto St. Andrew Street
9	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review
10	Signage and wayfinding strategy	Co-ordinated signage palette and types along St. Andrew Street. At Norco House and George Street junction, signage could be clearly positioned to direct to the Bon Accord Centre, NESCOL and further into the city centre.
11	Install rain gardens, SuDS and planting which filters out pollutants.	Footpath width decreased to allow for rain gardens / SuDS as part of streetscape softening
12	Street tree planting	Carriageway width balanced to include suitable pedestrian footpaths and tree planting to formalise on street parking and frame the streetscape
13	Carriageway redesign	<ul style="list-style-type: none"> <li>Widening carriageway to 6.2m for bus suitability</li> <li>Rebalancing the footway so it is evenly provided on both sides</li> <li>Formalised parking bays on both sides of street</li> <li>Repositioned bus shelters</li> <li>Retention of loading bays and parking where possible</li> </ul>
14	Street furniture and bin storage	New street furniture located around Norco House and bus stops



A change in texture and colour can visually narrow the carriageway and slow down traffic



Subtle changes in materiality visually differentiate the carriageway from footways



Rain gardens planted between the carriageway and footways soften the streetscape

Table 19 - St. Andrew Street Projects

## John Street (George Street to Loch Street)

This section of John Street, which is a secondary street, sees an introduction of a variety of different interventions such as street planting and carriageway narrowing, to help improve the quality of the streetscape. In particular to reduce the negative visual impact of street car parking and bin storage. The Masterplan proposes that on-street parking along the northern side of the street is formalised through new material build outs and with the introduction of street trees.

The street is also reconfigured to allow for more opportunities for additional planting, seating and commercial spill out spaces alongside a reduced footway width along the southern side of the street.

John Street (East of George Street)		
Secondary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	8.05m	Increased to 9.66m (to include street planting)
Typical road width	5.85m	Decreased to 3.2m
Footpath width	2.11 / 3.70	Increased to 2.20m / decreased to 2.20m (variable)
On Street parking	Y - Circa 6 on-street spaces + Approximately 33m of single yellow line off-peak parking	Y
Loading bays	Y	Y
Bus route and stops	N	N
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Paving slabs	Granite setts
Streetscape		
Refuse storage	Y	Y
Street planting	Y	Y - tree planting and rain gardens along both sides
Street furniture	Y	Y - various types of fixed and moveable

Table 20 - John Street Details

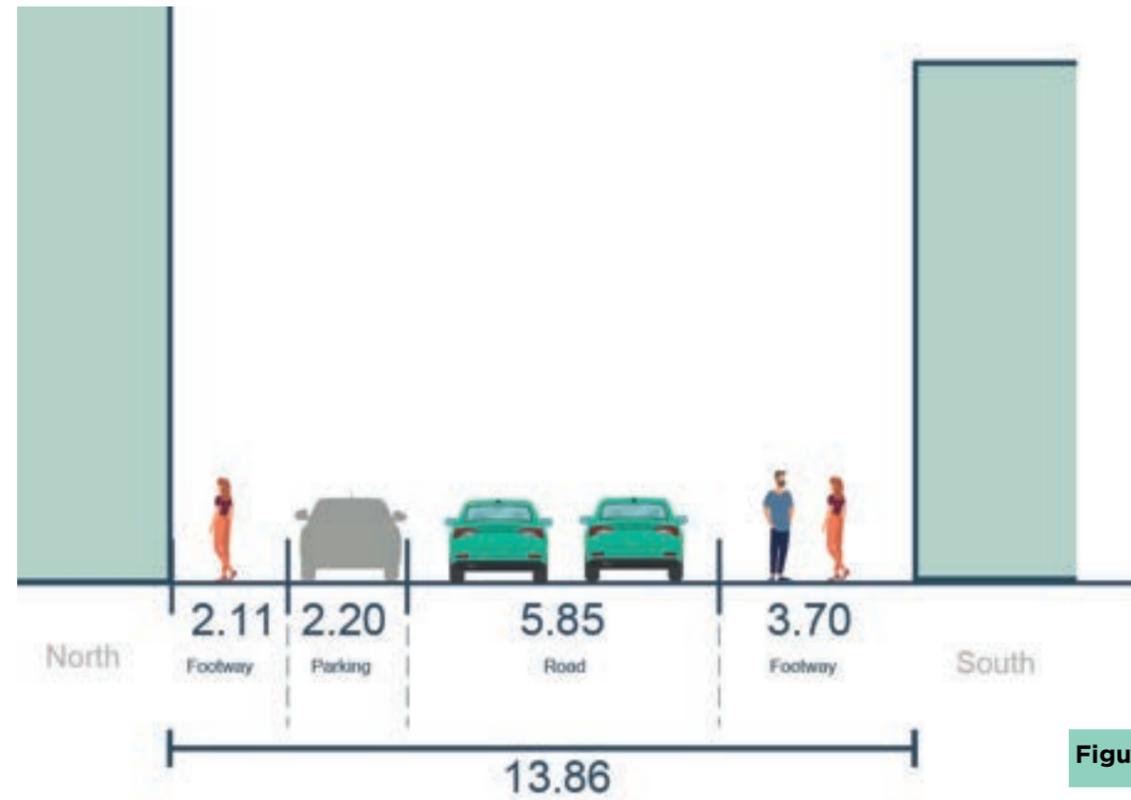


Figure 62 - Existing street section



Figure 63 - Proposed street section

Figure 64 - Concept plan of potential projects along John Street

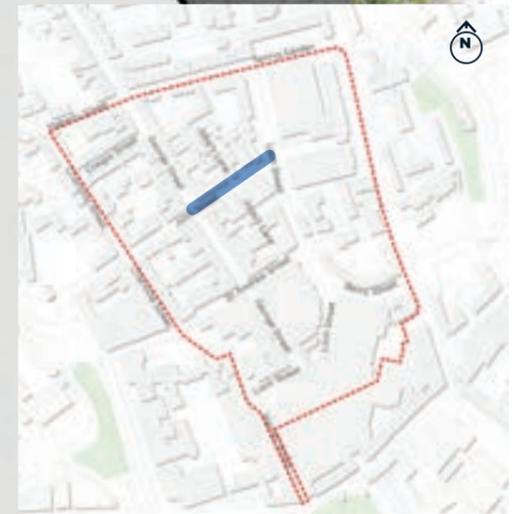


Figure 65 - Location within the Masterplan

## John Street (George Street to Denburn Road)

Similarly to the eastern extent of John Street, this section leading to Denburn Road will enhance the quality of the streetscape and environment through the introduction of new street trees, bin shelter storage and rain gardens in order to soften and rejuvenate this area of the Masterplan.

John Street is also widened to provide safer provision for active travel through mixed traffic streets and bus movements in both directions. Improved materials along the footway and better visual screening of bins will help to improve the overall aesthetic of the street. More formalised parking bays will also allow for the introduction of tree planting to help break up the street scene and introduce more colour.

John Street (West of George Street)		
Secondary Street		
	Current Design	Proposed Illustrative Design
<b>Street Design</b>		
Typical carriageway width	9.57m	Increased to 9.63m (to include street planting)
Typical road width	5.17m	Increased to 6.2m
Footpath width	2.33 / 1.53	Decreased to 1.8m / increased 1.80m (variable)
On Street parking	Y - Circa 15 on-street spaces	Y
Loading bays	Y	Y
Bus route and stops	Y	Y - Two way between George Street and Denburn Road
<b>Materiality</b>		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Paving slabs	Granite, tonal setts
<b>Streetscape</b>		
Refuse storage	Y	Y
Street planting	Y	Y - tree planting and rain gardens
Street furniture	Y	Y - various types of fixed and moveable

Table 21 - John Street Details

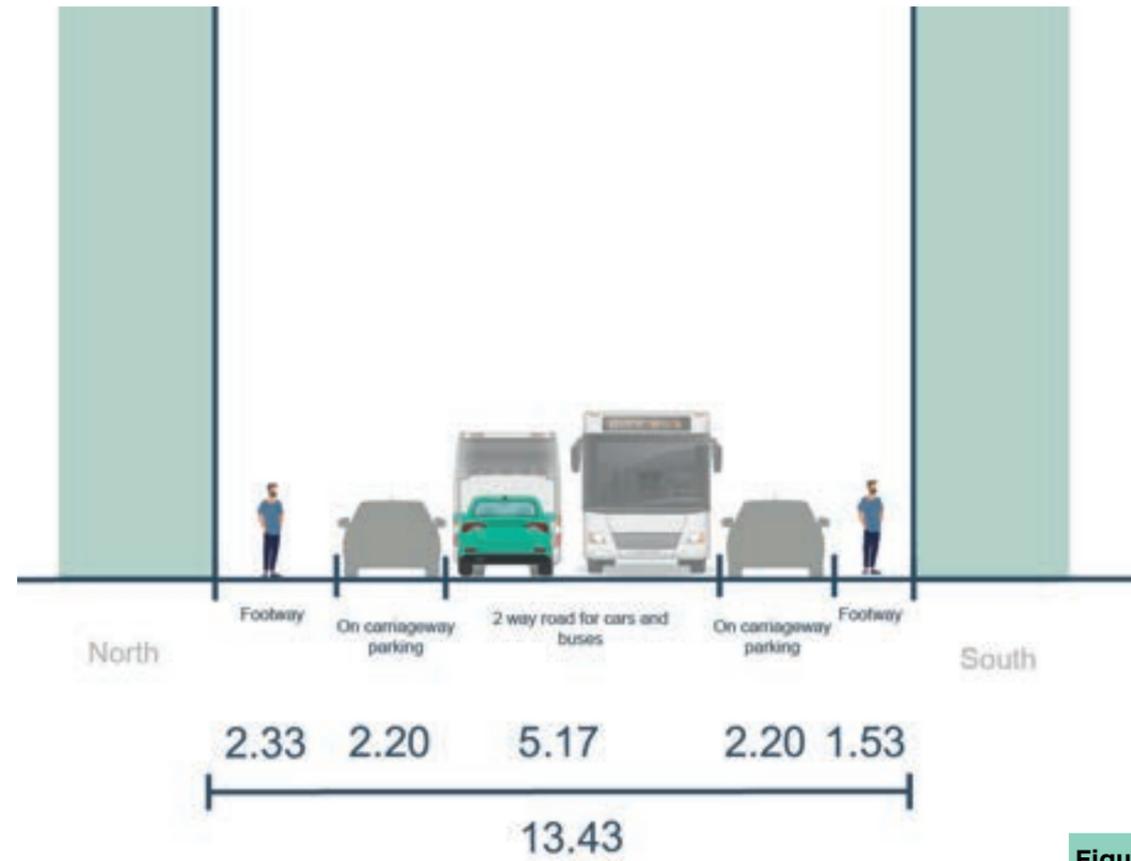


Figure 66 - Existing street section

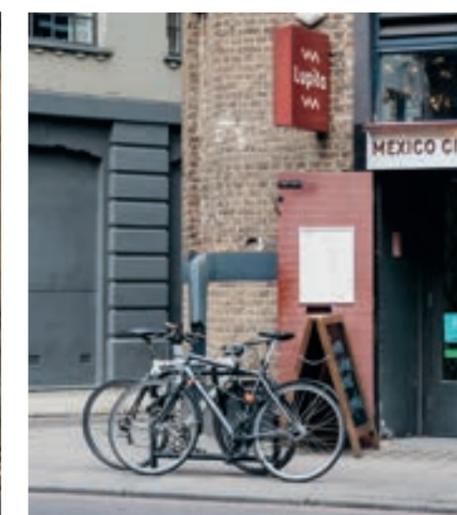
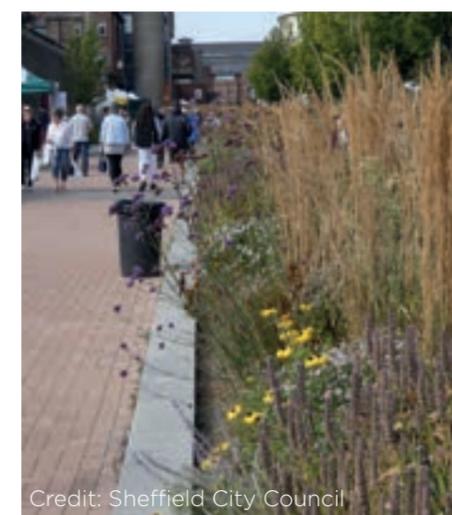


Figure 67 - Proposed street section

## John Street Projects (both east & west of George Street)

The following projects have been identified for John Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors onto John Street - painted, restored, highlighted through public realm materials
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Street & pedestrian crossing colouring	Focused on junctions with George Street, Jopp's Lane, Charlotte Street and Loch Street towards NESCOL
5	Improvement to quality and consistency of shop front signage	Working with local businesses to enhance and co-ordinate shop front signage to create degree of consistency
6	Bin storage	On-street bin storage is required on-street, provide shelters for screening
7	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
8	Uplighting and feature lighting to illuminate our special buildings	Sandman Hotel (both external uplighting and internal ground floor window lighting) as well as listed buildings. Also working with local residents to enhance their front doors onto John Street
9	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review
10	Signage and wayfinding strategy	Co-ordinated signage palette and types along John Street. At George Street and Loch Street junctions, signage could be clearly positioned to direct to the Bon Accord Centre, NESCOL and further into the city centre.
11	Install rain gardens, SuDS and planting which filters out pollutants.	Footpath width decreased to allow for rain gardens / SuDS as part of streetscape softening
12	Street tree planting	Carriageway width balanced to include suitable pedestrian footpaths and tree planting to formalise on street parking and frame the streetscape
13	Carriageway redesign	<ul style="list-style-type: none"> <li>Widening carriageway to 6.2m for bus suitability along the George Street to Denburn Road section</li> <li>Rebalancing the footway so it is evenly provided on both sides</li> <li>Formalised parking bays on both sides of street</li> <li>Repositioned bus shelters</li> <li>Retention of loading bays and parking where possible</li> </ul>
14	Street furniture and bin storage	New street furniture located around bus stops



**Table 22 - John Street Projects**

## Charlotte Street (John Street to Maberley Street)

Charlotte Street is the second of two residential streets within the Masterplan area which has a considerably different aesthetic to surrounding streets due to its materiality and residential offer, but also performs an important role in allowing traffic to travel along it, from Harriet Street and Spring Garden. The street is split into two, with a mainly residential character north of St. Andrew Street, with lower scale, historic and domestic building forms. Towards Robert Gordon's College, the street becomes more enclosed and the built form that defines it has less front doors onto street, making it feel less animated.

The street will continue to function as a residential street, however a key focus of transformation on this street is to slow traffic passing through, creating a resident and pedestrian focused environment. Pops of colour will add contrast to grey tones and new tree planting and landscape features within the street will create a more pleasant environment for local residents. This new character and aesthetic should be continued along the full length of this northern aspect of Charlotte Street.

Charlotte Street		
Tertiary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	8.88m	Remains 8.88m
Typical road width	3.88m	Increased to 4.88m to support cyclist accessibility
Footpath width	3.73m / 2.22m	3.73m / 2.22m
On street parking	Y circa 47 bays	Formalised parking bays with footway build outs.
Loading bays	N	N
Bus route and stops	N (no bus route on northern section of Charlotte St)	N (no bus route on northern section of Charlotte St)
	Y (bus route only applicable to southern section of Charlotte St)	Y (bus route only applicable to southern section of Charlotte St)
Materiality		
Carriageway	Setts	Setts
Footway	Paving slabs	Black Granite Slab / Dark grey Whinstone Slab
Streetscape		
Refuse storage	Y	Y
Street planting	Y	Y - intermittently on both sides of carriageway defining car parking spaces
Street furniture	N	Y - raised planters as seating

Table 23 - Charlotte Street Details

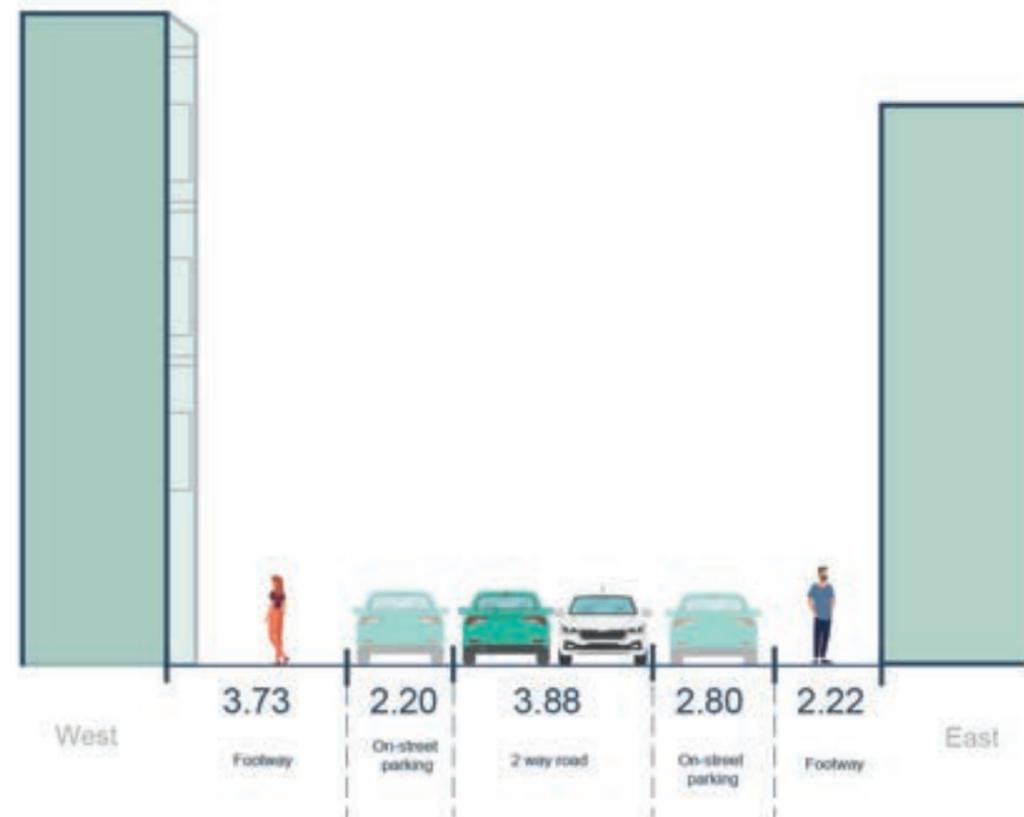


Figure 68 - Existing street section of Charlotte between John Street and Maberley Street

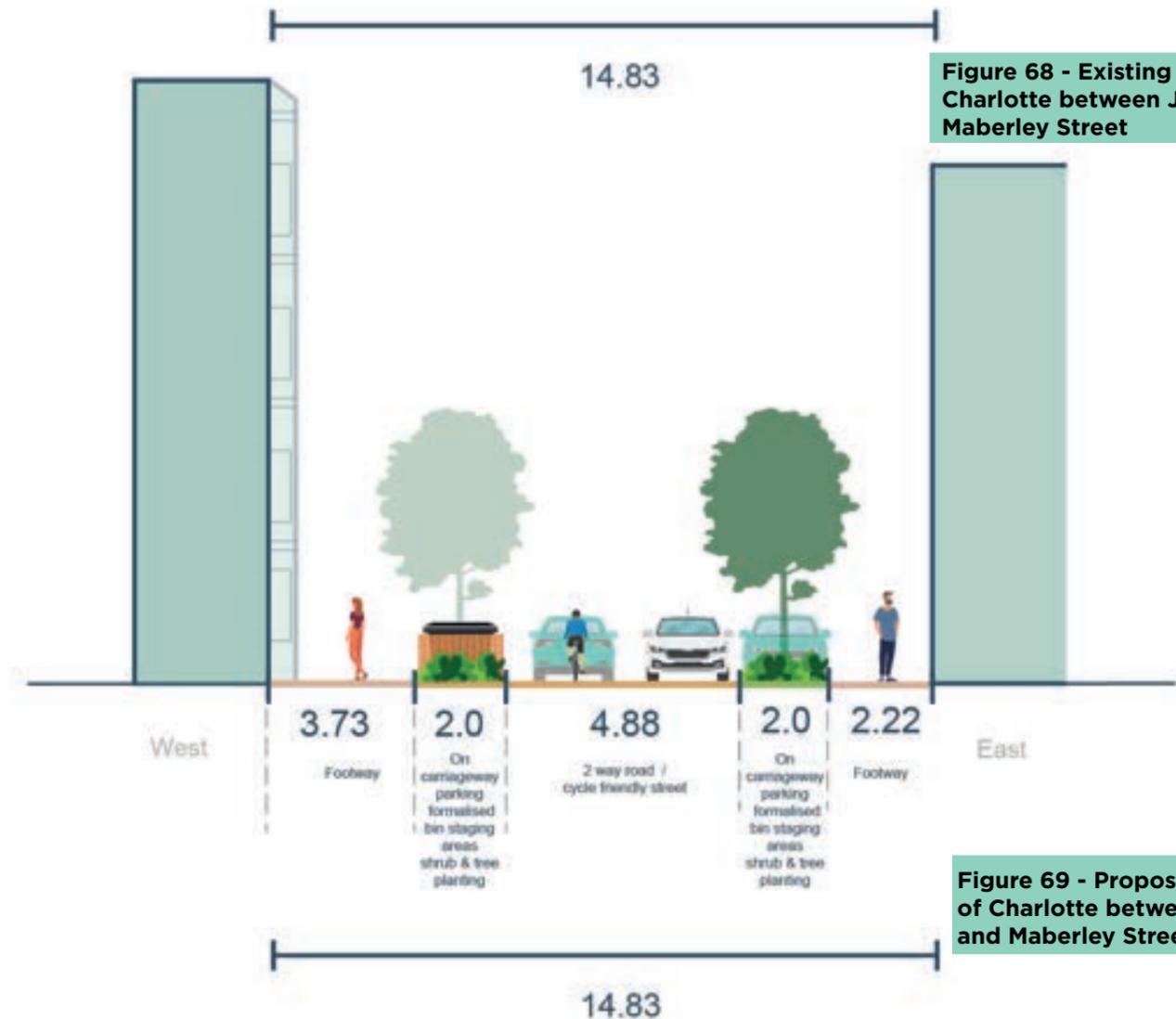


Figure 69 - Proposed street section of Charlotte between John Street and Maberley Street

# Charlotte Street Projects

The following projects have been identified for Charlotte Street which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, particularly along western side of the street, street signage and temporary signage to reveal built character
2	Residential front door enhancements	Working with local residents to enhance their front doors at the northern end of Charlotte Street - painted, restored, highlighted through public realm materials
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Bin storage	Where on-street bin storage is required on-street, provide shelters or visual screening
<b>Enhanced Place Experience</b>		
5	Front door lighting for residential tenements	Working with local residents to enhance their front doors onto Charlotte Street with wall mounted lighting
6	Signage and wayfinding strategy	Co-ordinated signage palette and types which links into the wider wayfinding strategy for the area.
7	Install rain gardens, SuDS and planting which filters out pollutants.	Footpath width decreased to allow for rain gardens / SuDS as part of streetscape softening
8	Street tree planting	Footpath width decreased to allow for street tree planting which defines on-street parking bays
9	Public spaces / residential grow spaces / play streets	Introduction of play street features
10	Carriageway redesign	<ul style="list-style-type: none"> <li>Rebalancing vehicle carriageway to support cyclist accessibility</li> <li>Footway realignment on both sides of street</li> <li>Junction radii tightening at junction with Charlotte Street &amp; John Street / Maberley Street</li> <li>Planter build outs slow traffic movement</li> <li>Formalised parking bays on both sides of street</li> </ul>
<b>George Street As A Destination</b>		
11	Improved CCTV coverage	At suitable locations e.g junction with John Street
12	Improved street lighting	New wall mounted and / or lighting columns within the carriageway
<b>Reducing Vehicle Dominance</b>		
13	More flexible use of on-street parking spaces - creating PARKlets which reclaim parking bays for socialising, play and dwell time.	Spaces to be identified with local residents. Interventions could be permanent or temporary depending upon approach. Opportunity to link with future community events / festivals etc across the area.

**Table 24 - Charlotte Street Projects**



**Figure 70 - Concept plan of potential projects along northern section of Charlotte Street**



**Figure 71 - location within the Masterplan**



Subtle road alignments can help manage traffic speeds in residential areas



Creating comfortable pedestrian routes and rain gardens / soft planting features making better use of space available

## Jopp's Lane

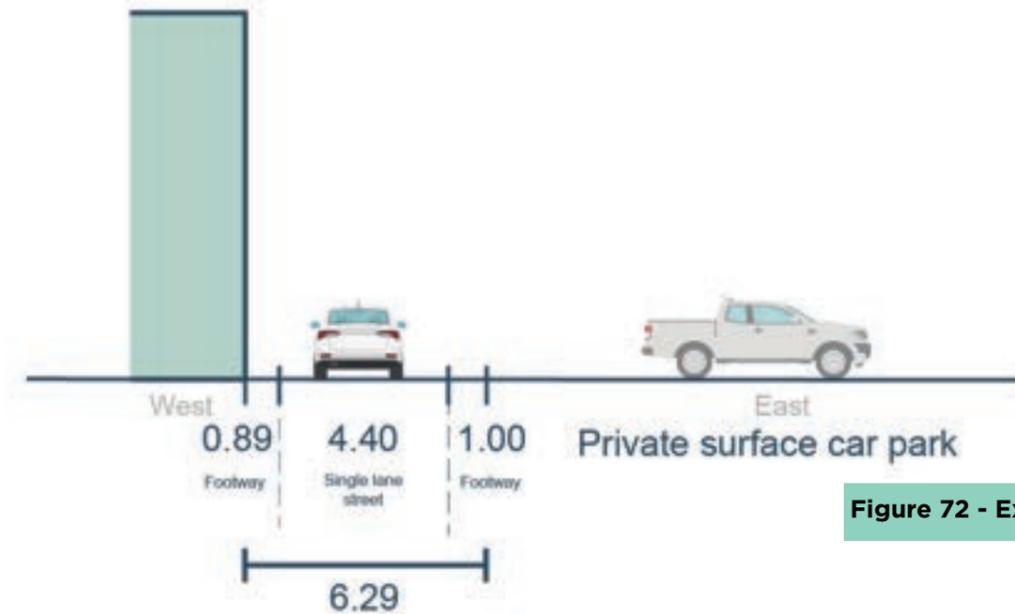
Jopp's Lane is a tertiary street, has a intimate and quiet character and is home to a number of independent businesses. It is also where a number of NuArt mural works are located. It accommodates pedestrians however it is predominately used as a service access with some cars using the lower section to move through to get to their next destination.

Jopp's Lane will become a point of interest within the area, defined by a character and aesthetic than entices people to visit and explore. It will retain its informal character and become more of a shared surface route, with delineated footways, prioritising pedestrians in the area and offering a different sense of place. Colourful street furniture, vertical greening and sensory planting, festoon lighting and bold super graphics reinforce its connections with the NuArt festival and will allow people to more comfortably enjoy the character and visual interest along this unique street within the Masterplan.

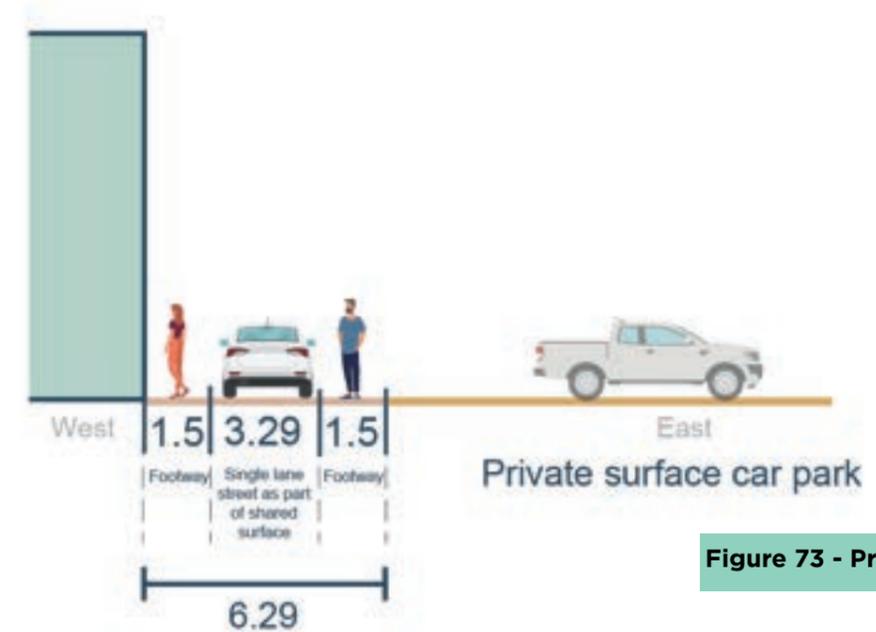
The combination of proposed projects for Jopp's Lane will help to create a genuinely characterful and enjoyable environment that supports George Street's creative and artistic identity.

Jopp's Lane		
Tertiary Street		
	Current Design	Proposed Illustrative Design
<b>Street Design</b>		
Typical carriageway width	4.40m	Decreased to 3.29m
Typical road width	4.40m	Decreased to 3.29m
Footpath width	1m / 0.89m	Redefined to 1.5m / 1.5m
On Street parking	N	N
Loading bays	N	N
Bus route and stops	N	N
<b>Materiality</b>		
Carriageway	Hot rolled asphalt	Granite tonal setts
Footway	Small concrete slabs & setts	Black Granite Slab / Dark grey Whinstone Slab
Kerb	Granite	Flush, mixed coloured granite
<b>Streetscape</b>		
Refuse storage	N	Y
Street planting	N	Y - moveable planters
Street furniture	N	Y - within footway provision

**Table 25 - Jopp's Lane Details**



**Figure 72 - Existing street section**



**Figure 73 - Proposed street section**

# Jopp's Lane Projects

The following projects have been identified for Jopp's Lane which will contribute to securing the Place Objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of rubbish, weeds and overgrown planting,
3	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
4	Street & pedestrian crossing colouring	Encouraging mural painting and street painting across Jopp's Lane to create a fun and creative environment to walk through. Continuing this painting across John Street would connect both ends of Jopp's lane.
5	Improvement to quality and consistency of shop front signage	Working with local businesses to enhance and co-ordinate shop front signage to create degree of consistency that links back to the wider area strategy
<b>Enhanced Place Experience</b>		
6	Festoon lighting / lighting net	Festoon lighting located along both sections of Jopp's Lane. Opportunity to introduce distinctive and memorable lighting interventions to create a strong identity for Jopp's Lane
7	Create active uses and frontage	Temporary uses within derelict / vacant buildings to the north of Jopp's Lane, could include pop up art exhibitions, bars and restaurants to attract footfall into the area.
8	Signage and wayfinding strategy	Co-ordinated signage palette and types that link back to George Street. Signage could be positioned at John Street and St. Andrew Street.
9	Carriageway redesign	<ul style="list-style-type: none"> <li>Shared surface route</li> <li>Reduced vehicular route width</li> <li>Junction radii tightening at junction with John Street and St. Andrew Street</li> <li>Distinctive crossing point at John Street and St. Andrew Street</li> <li>New palette of materials</li> <li>Inclusion of sensory planting at junction with John Street to support nearby uses</li> </ul>
<b>George Street As A Destination</b>		
10	Visitor and residential information points - these could be pop up kiosks / interactive stands / live cycle counters / air quality readings etc	Encouraging street art on blank façades along Jopp's Lane will create a unique character for the area and make the area 'instagramable'. Art Streets create pockets of interest where people are encouraged to take and share photos on social media.
11	Improved CCTV coverage	At suitable locations e.g John Street and St. Andrew Street junctions.

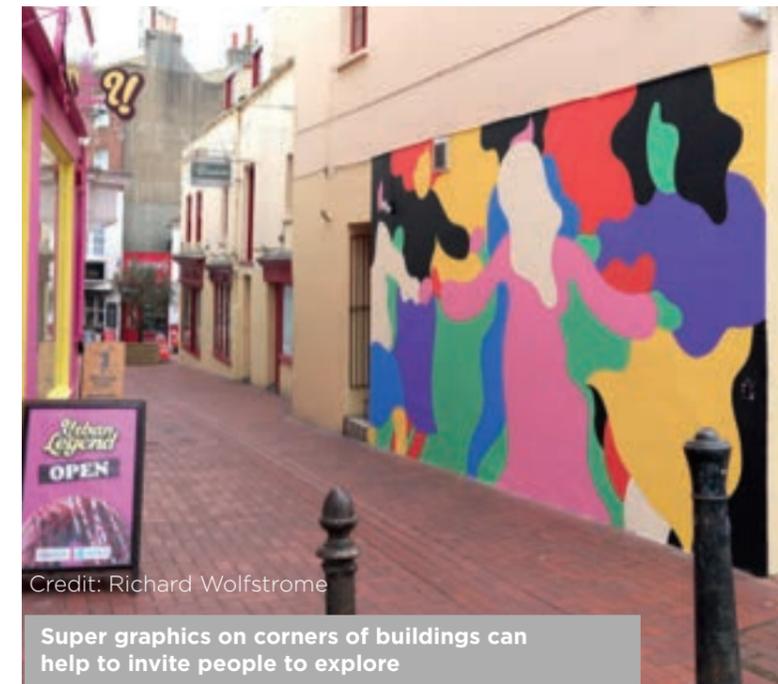


**Figure 75 - Location within the Masterplan**

**Figure 74 - Concept plan of potential projects along Jopp's Lane**



Catenary lighting creates a playful streetscape



Credit: Richard Wolfstrom

Super graphics on corners of buildings can help to invite people to explore



Credit: Richard Wolfstrom

Vertical greening helps boost the biodiversity within the area

**Table 26 - Jopp's Lane Projects**

## Spring Garden

Spring Garden is a peripheral route to the northern boundary of the Masterplan area, however it is a key primary street for vehicular movement from Skene Square to Gallowgate.

Spring Garden will continue to be a two way vehicular route, however the corridor will be rebalanced to create a greener environment with improved pedestrian footway provision.

Carriageway reconfiguration will provide street tree planting along its northern side and introduce rain gardens and boundary planting to help create a more pleasant environment for pedestrians and residents. Additions of colour and signage at key junctions such as Loch Street and George Street will help to create a clear and comfortable journey to NESCOL and George Street.

A 2.0m wide segregated cycle lane both east and westbound between the junction with Gallowgate and the junction with Loch Street would be included, with the rest of Spring Garden functioning as a mixed traffic street. This will allow for a safe cycle connection from the NCN1 to NESCOL.

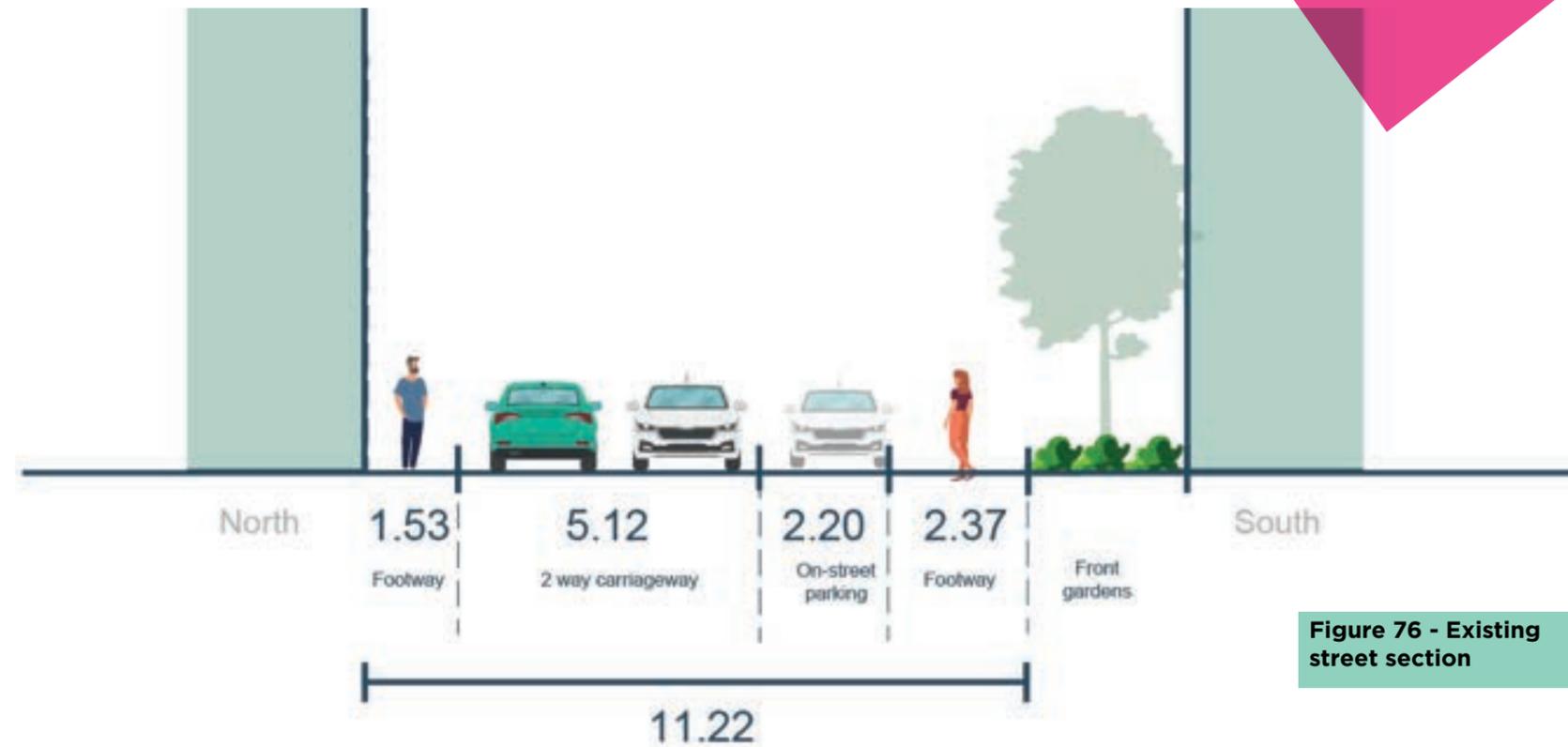


Figure 76 - Existing street section

Spring Garden		
Secondary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	7.32	Decreased to 7.1m
Typical road width	5.12m	Increased to 5.6m
Footpath width	1.53m / 2.37m	Resized to 2m / 2.12m
On Street parking	Y (circa 9 no. of spaces / 1 accessible space)	Potential for existing parking for residents to be included within current courtyard parking provision. Potential loss of spaces
Loading bays	N	N
Bus route and stops	N	N
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Paving slabs	Dark grey Whinstone slabs
Streetscape		
Refuse storage	N	N
Street planting	Y	Y - on one side of carriageway
Street furniture	N	Y - within footway provision

Table 27 - Spring Garden Details

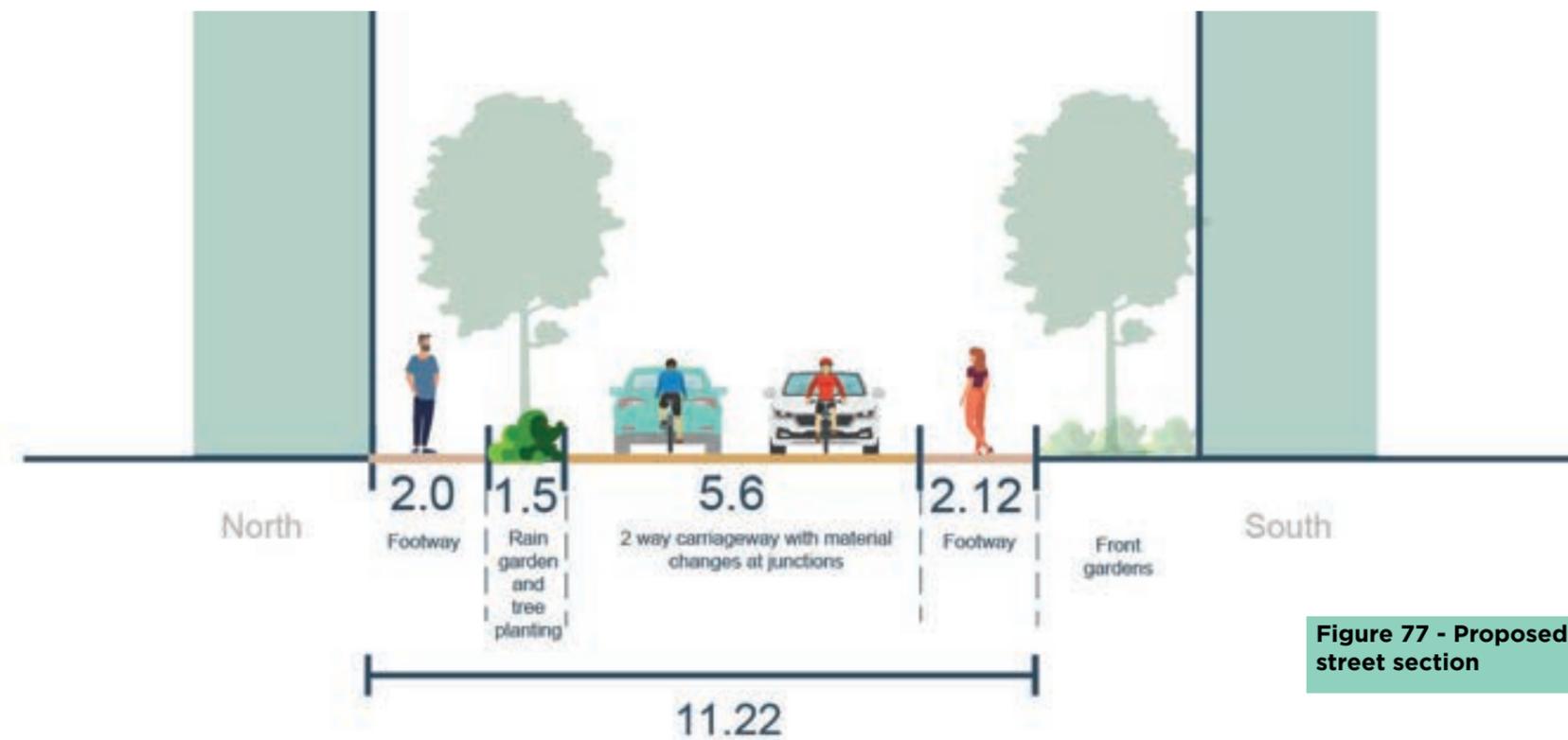


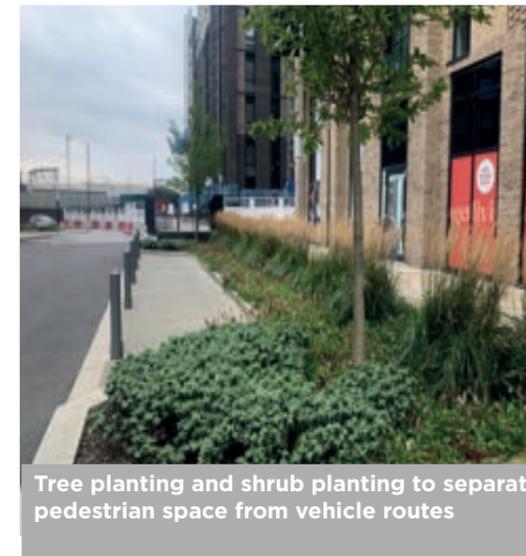
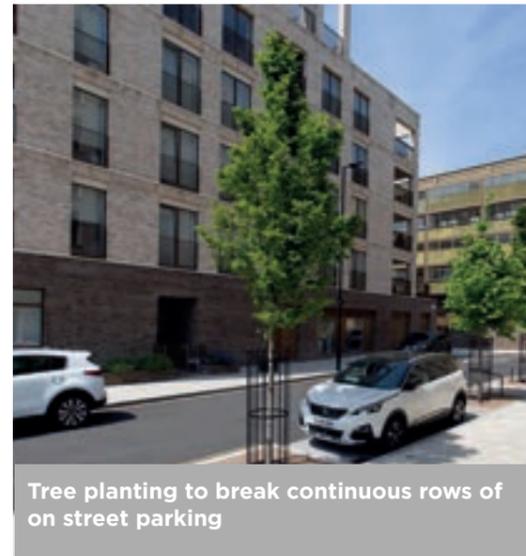
Figure 77 - Proposed street section

# Spring Garden Projects

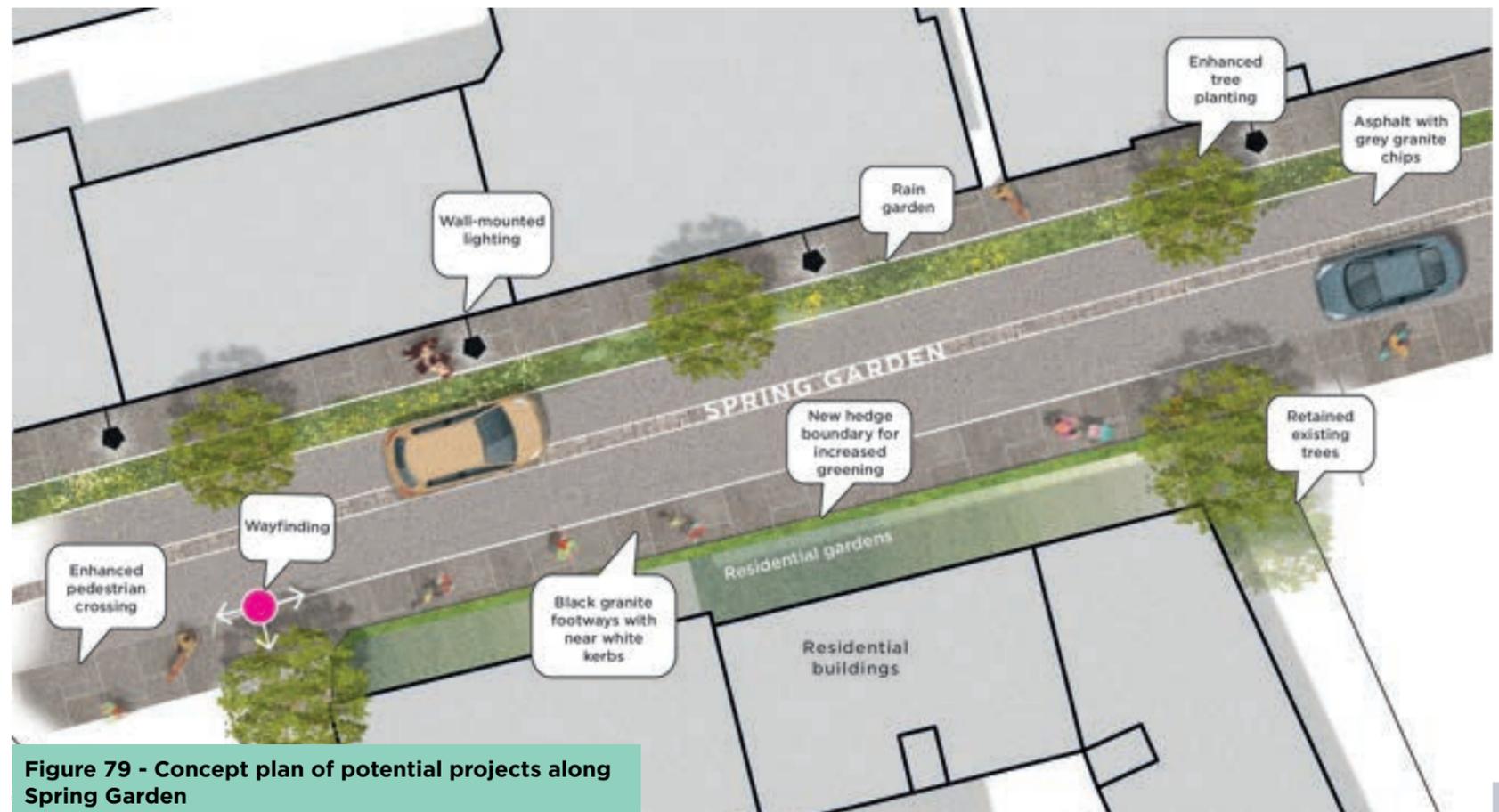
The following projects have been identified for Spring Garden which will contribute to securing the 'Place' objectives of the Masterplan.

Ref:	Project	Description
<b>Appearance and Character</b>		
1	Removal of clutter	Improvements to streetscape focused on removal of redundant satellite dishes, street signage and temporary signage to reveal built character
2	Deep clean of existing paving, street furniture and building façades	Interim works whilst longer term streetscape enhancements are developed
3	Street & pedestrian crossing colouring	Focused on junction with Loch Street to make the area more attractive to students and visitors to NESCOL. This would help connect Loch Street to Spring Garden so the area reads as one environment around the college.
4	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
5	Front door lighting for residential tenements	Working with local residents to enhance their front doors onto Spring Garden
6	Create active uses and frontage	Temporary uses within derelict buildings - specific buildings to be identified / kept under review
7	Signage and wayfinding strategy	Co-ordinated signage palette and types along Spring Garden footways. At NESCOL and George Street junctions, signage could be clearly positioned to direct to the college and the city centre.
8	Install rain gardens, SuDS and planting which filters out pollutants.	Footpath width decreased to allow for rain gardens / SuDS as part of streetscape softening
9	Street tree planting	Carriageway width balanced to include suitable pedestrian footpaths and tree planting to formalise on street parking and frame the streetscape
10	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway</li> <li>Rebalancing the footway so it is evenly provided on both sides</li> <li>Junction radii tightening at junction with Loch Street, and at access points to residential car parks. This could include crossing build outs at these junctions</li> <li>Formalised parking bays on both sides of street</li> </ul>
11	Street furniture and bin storage	New street furniture located around NESCOL
<b>George Street As A Destination</b>		
12	Visitor and residential information points	These could be located at Loch Street junction and George Street and could include interactive stands / live cycle counters / air quality readings etc
13	Improved CCTV coverage	At suitable locations e.g Gallowgate, Loch Street and George Street

**Table 28 - Spring Garden Projects**



**Figure 78 - Location within the Masterplan**



**Figure 79 - Concept plan of potential projects along Spring Garden**

# Gallowgate

Gallowgate is a primary and NCN route to the east of the Masterplan boundary, and a key connector for traffic movement to and from the City Centre.

Gallowgate will continue to be a two way vehicular route. It currently has informal cycle lane provision along it and there is an opportunity to introduce improved cycle lane provision along the corridor, reinforcing its NCN status and connectivity to the City Centre. This could include a segregated 2.0m cycle lane in both directions as far as the existing street section will allow.

Although the existing carriageway is wide, the functional nature of this corridor limits the opportunity for the new street planting. Footways are adjusted to 2.0m to create safe and comfortable journeys and to allow for a segregated 2.0m cycleway in both directions, as far as possible towards Upperkirkgate. A narrow SuDS / rain garden is included along the western side of the street section to provide separation from the footpath and cycle lane. Cycle route will need to be designed around existing bus stops along Gallowgate. The proposals would require the loss of the existing on-street parking bays, however there is provision within close proximity both along Loch Street and within the nearby MSCP.

Gallowgate		
Primary Street		
	Current Design	Proposed Illustrative Design
Street Design		
Typical carriageway width	10.64m	Increased to 11.20m
Typical road width	8.44m	Decreased to 6.20m
Footpath width	1.62m / 3m	Resized to 2m on each side
On Street parking	Y (circa 12 no. of spaces at NESCOL)	Loss of parking spaces to accommodate cycle infrastructure
Loading bays	N	N
Bus route and stops	Y	Y
Cycle lane	Y	Y - enhanced to provide segregation in both directions
Materiality		
Carriageway	Hot rolled asphalt	Hot rolled asphalt
Footway	Paving slabs	Dark grey Whinstone slabs
Streetscape		
Refuse storage	N	N
Street planting	Y	Y - on one side of carriageway
Street furniture	N	Y - within footway provision

Table 29 - Gallowgate Details

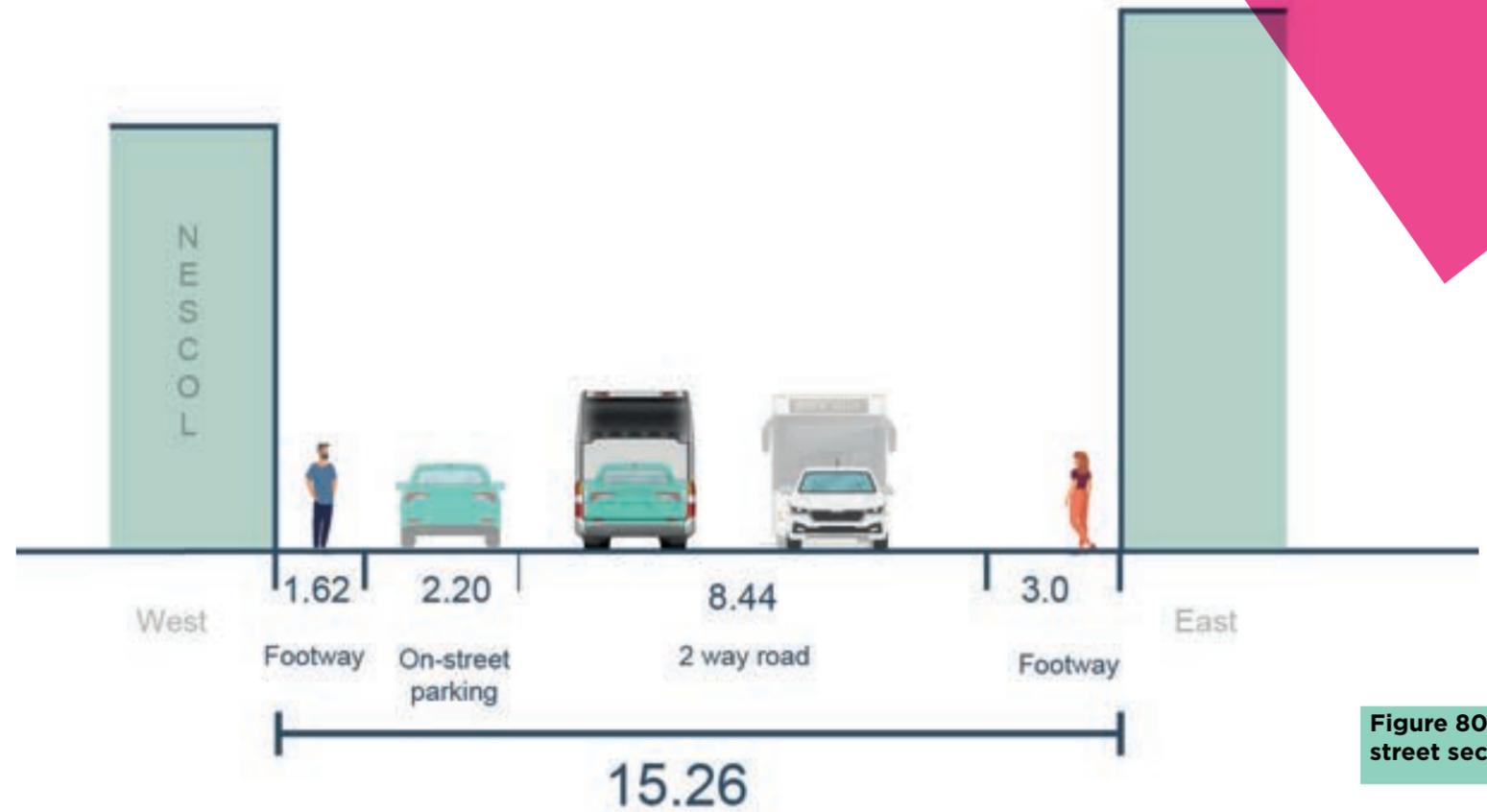


Figure 80 - Existing street section

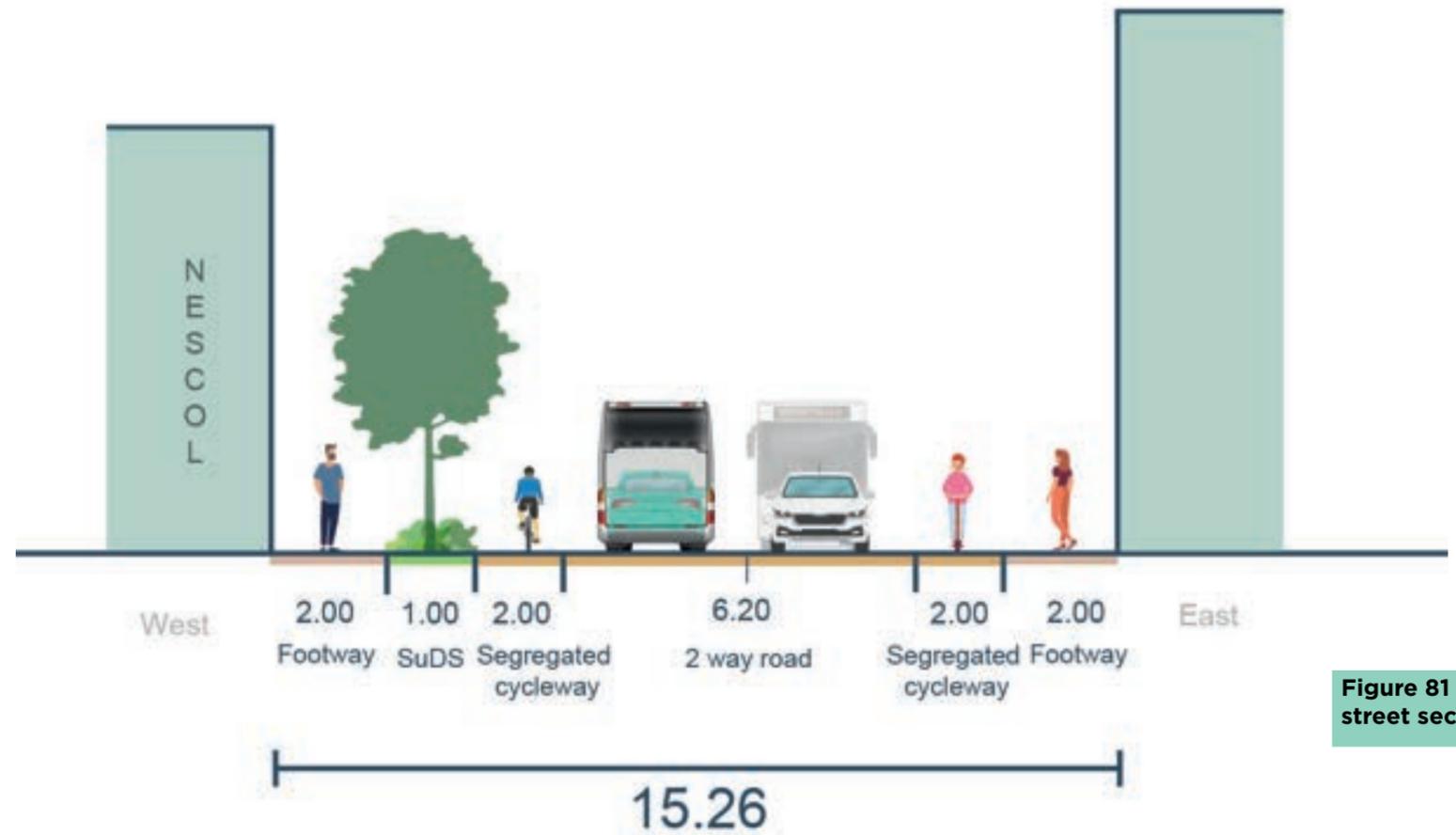


Figure 81 - Proposed street section

# Gallowgate Projects

The following projects have been identified for Gallowgate which will contribute to securing the 'Place' objectives of the Masterplan.

Ref.	Project	Description
<b>Appearance and Character</b>		
1	Deep clean of existing paving, street furniture, bus stops and building façades	Interim works whilst longer term streetscape enhancements are developed
2	Refurbish and restore existing street bins, bollards, cycle stands	Interim works whilst longer term streetscape enhancements are developed
<b>Enhanced Place Experience</b>		
3	Front door lighting for residential tenements	Working with local residents to enhance their front doors towards Loch Street and Upperkirkgate
4	Signage and wayfinding strategy	Co-ordinated signage palette and types along footways. At NESCOL and both Berry Street and Spring Garden junctions, signage could be clearly positioned to direct to the College and the city centre.
5	Street tree planting	Carriageway width to include suitable pedestrian footpaths and SuDS features to manage stormwater and introduce biodiversity
6	Carriageway redesign	<ul style="list-style-type: none"> <li>Narrowing vehicle carriageway</li> <li>Rebalancing the footway so it is evenly provided on both sides</li> <li>Segregated cycle lane for National Cycle Route 1 in both directions</li> <li>SuDS features included</li> </ul>
7	Street furniture and bin storage	New street furniture located around NESCOL and bins
<b>George Street As A Destination</b>		
8	Visitor and residential information points	These could be located at Spring Garden and Berry Street junctions and could include interactive stands / air quality readings / daily cycle counts along NCN routes etc
9	Improved CCTV coverage	At suitable locations e.g Gallowgate, Spring Garden and Berry Street and around entrances to key buildings
10	Improved WiFi coverage	To extend to areas around NESCOL for students to use externally as part of improved dwell time opportunities

**Table 30- Gallowgate Projects**



Introduction of signage and wayfinding as part of an improved NCN cycle route



A segregated cycleway along Gallowgate in both directions will give cyclists a safe and designated route to and from the city centre, minimising interaction with vehicles.

# Transformational Project 4

## Bon Accord Centre

### Overview

The Bon Accord Centre is a fundamental part of the George Street Masterplan area, because of its physical presence, how it influences the movement of people into and out of George Street and the goods and services it offers.

Since its construction in the 1990s it has reshaped George Street's relationship with Aberdeen city centre. Historically George Street served as a direct arterial route to Union Street and further south to the harbour.

However, the 3-storey building, whose layout capped the southern end of George Street, severed its direct physical connection to the city centre and from within the Masterplan area it is difficult to perceive the proximity of the heart of the city centre.

The northern elevation is defined by granite blockwork, ramped level changes, opaque glazed panels above the entrance. Wrapping along its northern and eastern elevations are brise soleil panels combined with granite blockwork to screen the multi-storey car park. The elevation running to the west towards and along Harriet Street is defined mainly by granite blockwork, service access yards and entrance and exit ramps from the multi-storey car park.

In its current form there is no animation to the elevations both at ground floor (in terms of shops / uses looking onto the street) and above to the roof (in terms of colour / screening to help add character to the frontage). Pedestrians can still continue towards Upperkirkgate, travelling through the centre, only during opening hours.



### Opportunities to enhance:

Given the prominence of the Bon Accord Centre to George Street, and its surrounding streets, the Masterplan identifies a number of opportunities to improve the visual and functional use of the facade. The Bon Accord Centre is not within Council ownership and the following projects are suggested as a guide in line with the Vision and objectives of the Masterplan. Ultimately the ability and scope of delivery would be determined by the owners of the Bon Accord Centre.

Possible interventions could include:

#### 1. Facade treatments

These could include a series of temporary or permanent features to completely change the look and feel of the building's elevations. Not only could this help the centre better integrate with the other transformational projects identified within the Masterplan, it would also help to promote the location of the centre itself, helping to visually establish it within the wider city centre. Opportunities to expand the NuArt festival further onto the building could be explored, as well as coloured cladding, living walls, bold colours added and retro-fitted panels.

#### 2. Illumination

Linked to the creation of a new public space along Loch Street and at Norco Place, there could be an opportunity to project onto parts of the facade. This could include animation, artwork or even as part of an outdoor cinema. In addition there could be opportunities to creatively uplight the facade of the building to help bring colour, accentuating the reveals and depressions in the elevation, adding character and interest.

#### 3. Activation

Opportunities to activate the ground floor of the northern elevation onto Loch Street and Berry Street should be explored. This could complement the proposed new public space along Loch Street and bring vibrancy to Loch Street. Uses could diversify beyond retail to more food and beverage and leisure focused to bring life to this part of the Masterplan beyond 5pm. Pop-up markets or

more permanent reconfigurations to ground floor service areas, both below the existing access ramps and above, could create space for both new commercial uses and dwell space for new cafe's.

#### 4. Openings

There is potential to enhance the arrival experience to the Bon Accord, south along George Street, by re-configuring the building entrance onto George Street. Increasing the apertures of the building, updating the entrances with modern material palette, raising the ground floor door openings and replacing the opaque glass with clearer glass will create a more welcoming and inviting approach. Light will penetrate deeper into the building improving the internal experience as well as allowing for more through views, allowing people to see what activity is happening inside.

#### 5. Longer term

There may be opportunities in the future to consider significant changes to the Bon Accord Centre, should market conditions be favourable. Large scale retail footprints are having to adjust and adapt to changing retail trends, with some completely moving towards a finer grained, more mixed use offer. Changes often include breaking the large scale of the buildings down, creating smaller units that are easier to let, as well as creating more pedestrian routes through. Future opportunities should be explored to better connect George Street to Upperkirkgate.

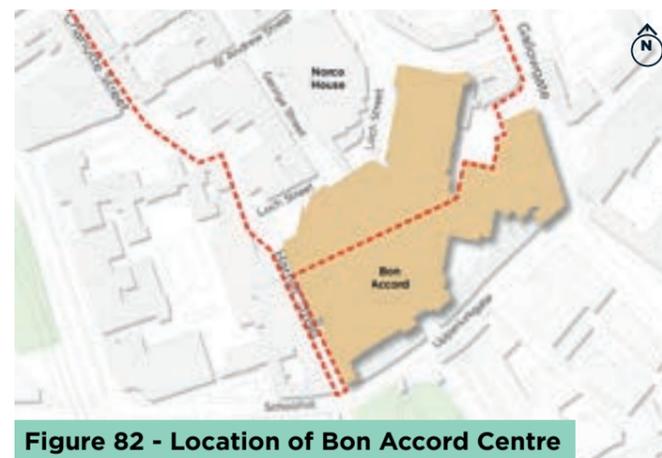
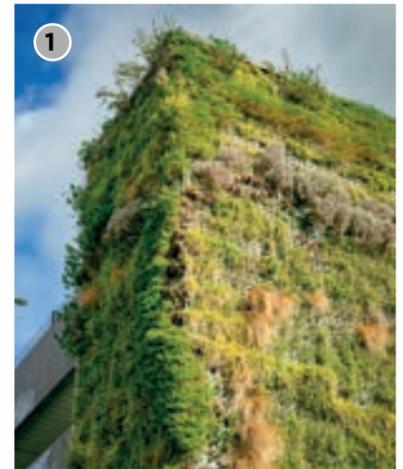


Figure 82 - Location of Bon Accord Centre



Opportunity to project onto parts of the facade. This could include animation, artwork or even as part of an outdoor cinema hosted within a new pocket park. Service access would still be provided but this could be part of a temporary pop up use.

Opportunities to creatively uplift the facade of the building to help bring colour, accentuating the reveals and depressions in the elevation, adding character and interest.

A more visually connected public realm with consistent materiality guides people down to an enhanced Bon Accord Centre entrance.



Either through internal reconfiguration or retrofit to create new activities onto Loch Street that help to bring activity, vitality and stronger fronts.

Alteration to elevation of the Bon Accord Centre along Loch Street with potential for single storey cafe/restaurant/bar unit.

Opportunities to expand the NuArt festival further onto the building could be explored, as well as coloured cladding, living walls, bold colours added and retro-fitted panels.

Facade alteration to Bon Accord Centre to allow for better visibility into the shopping centre.

**Figure 83 - Concept image of Bon Accord Centre facade and street uses**

# Transformational Project 5

## Redevelopment Opportunities

The Masterplan identifies a number of potential opportunity sites, both those that have a) live or ongoing planning interest and b) longer term potential redevelopment / investment site. Given that some sites have approved (but not yet commenced) applications, these have been identified as longer term opportunity sites.

The longer term potential redevelopment / investment sites are identified on the basis that the current form, appearance and/or quality of the building and its surrounding environment could benefit from investment or redevelopment, allowing them to better integrate with the 'place' proposals. The future of these sites will be determined by the suitability and opportunity presented by the market and the intentions of the asset / site owner.

### A) Hilton Garden Hotel - St. Andrew Street

Opportunity to upgrade / refurbish / redevelop existing building and car parking area to rear to bring active frontage along St. Andrew Street. The site had an application refused and remains an important opportunity site along St. Andrew Street.

### B) Norco House - George Street

Further detail as a transformational project. Opportunity to create landmark anchor through re-development / retrofit for mixed use.

### C) Balaclava Bar, vacant units, former Nickel and Dime store and rear service yards / surface car park - George Street / Loch Street

Opportunity to create positive built form and greater massing along Loch Street / Crooked Lane, bringing ground floor active uses to a currently underutilised part of the Masterplan. Re-development in this location could have a positive, transformational impact on the arrival experience travelling north along Harriet Street, arriving to a new, high quality buildings and new uses.

### D) Surface car park sites - Crooked Lane

Opportunity to realise development on current car park sites which occupy visually prominent locations on approach to George Street eastbound along St. Andrew Street. New development, at a considered scale, which addresses the entrance to Robert Gordon's College could help to create a better defined nodal point at this important junction.

### E) Broadford Works -

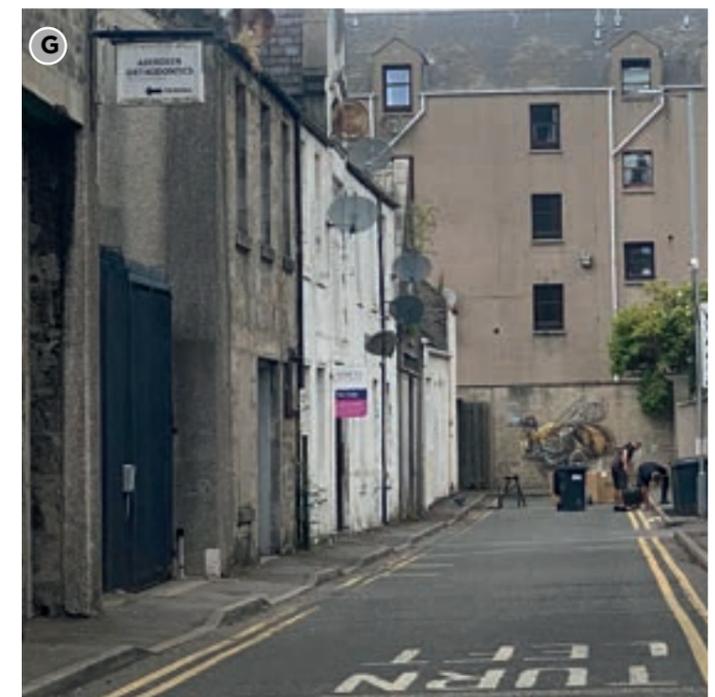
sitting just beyond the Masterplan boundary but representing a significant influence to the neighbourhood will be the realisation of Broadford Works. This could create a significant new residential community that will influence redevelopment opportunities across the Masterplan and begin to establish a new development context.

### F) Large format retail units, surface car park and bike parking - Loch Street

Longer term potential to reconsider building format and parking arrangements in this location, to better utilise the space available in a more efficient plot layout. Introducing a greater scale and density on the plot, suitable to George Street, could help to reinforce Loch Street junction and improve arrival experience to NESCOL. Current cycle and motorcycle parking on the site could be re-provided as part of the NESCOL site opportunities.

### G) Vacant warehouse - Jopp's Lane

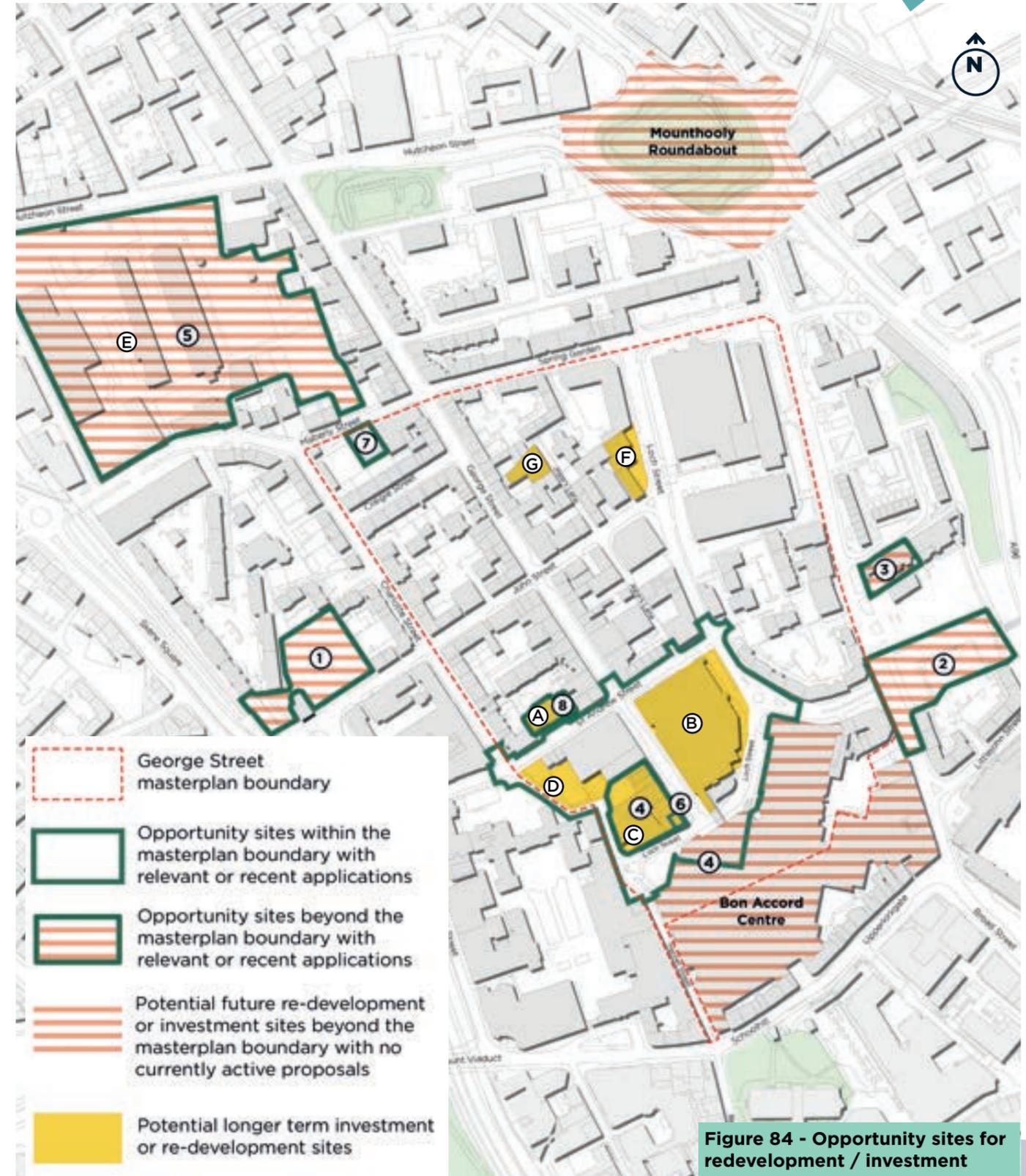
Opportunity to refurbish existing warehouse building for variety of potential uses bringing footfall and activity to Jopp's Lane



The table below summarises the planning status of sites that have submitted applications within or near to the Masterplan area, some of which are redevelopment opportunity sites.

Site Ref	Location	Status	Potential use	Application ref
1	92-126 John Street	Pending determination	Erection of student accommodation (circa 383 beds) with associated infrastructure and landscaping.	230514/DPP
2	Greyfriars House, Gallowgate	Approved	Residential Development (circa 140 units) including demolition of existing building and associated access, parking, landscaping and infrastructure works.	200246/PPP
3	28 Gallowgate	Withdrawn	Potential for residential or student accommodation	N/A
4	Bon-Accord Centre, George Street	Approved (but not commenced)	Mixed use development comprising Class 1 (Shops), Class 2 (Professional services), Class 3 (Food and drink), Class 4 (Business), Class 7 (Hotels), flats, serviced apartments, access, services and all ancillary development and associated demolitions.	170353/PPP
		Approved (but not commenced)	Variation of conditions 3 (approval of drawings), 6 (details of green wall), 7 (lighting scheme), 8 (landscaping) and 9 (external finishes) of planning permission P141192 in order to enable implementation of works associated with proposed cinema and restaurant terrace.	171136/S42
		Approved (but not commenced)	Formation of cinema within existing retail units and other associated works.	200101/PRE
		Certificate Issued	Proposed use of existing consented bowling centre (Class 11) as a family entertainment centre (within Class 11).	221243/CLP
5	Broadford Works - Maberly St	Approved (but not commenced)	Residential led (c.890 units) mixed use development, incorporating conversion of Listed Buildings. Named uses include apartments for rent; student residential accommodation; leisure; crèche/nursery; restaurant; cafe and bars; office; retail; concierge facilities; reuse of "Grey Mill" building as studio workshop, live/work and gallery space; public realm; car parking; and ancillary works.	160150/PPP
6	93-99 George Street	N/A	Proposed creation of 7 residential flats over existing class 2 (bank) which is currently pending a change of use application to a class 3 (restaurant)	220767/PREAPP
7	15 Maberly Street	Approved (but not commenced)	Redevelopment of an existing site for erection of 17 residential flats over 4 storeys including demolition and all associated works	200621/DPP
8	31 St. Andrew Street	Refused	Change of use from class 7 (hotels and hostels) to student accommodation (sui generis)	211263/DPP

**Table 31 - Opportunity Site Details**



**Figure 84 - Opportunity sites for redevelopment / investment**

# Transformational Project 6

## NESCOL

NESCOL is an important educational institution located within the Masterplan area. It supports a variety of students offering both day and evening classes. Along with Robert Gordon's College, they attract high numbers to the area on a daily basis and the Masterplan seeks to ensure that students, staff, families, visitors, coming to the area are encouraged to make the most of George Street.

There are opportunities to be explored within and around the curtilage of NESCOL, to better integrate with the area and encourage more frequent trips between George Street and both institutions.

The 10 storey building and the single storey surrounding buildings occupy an important corner plot along Loch Street, Spring Garden and Gallowgate within the Masterplan area. The building is accessed from both the Gallowgate and Loch Street, with the Loch Street access located approximately 40m back from the street. The northern and western extents of the plot are occupied by surface car parking and defined by a low brick wall with railings.

The potential improvements proposed to Loch Street create an opportunity for the College to reconfigure its plot and boundaries to fully integrate with the surrounding environment. Potential opportunities to be explored could include;

### 1) Boundary interface with Loch Street

The existing low wall and metal rail fencing is tired and sets a defensive edge to the College grounds. Alongside the Loch Street streetscape enhancements, the boundary of the college could be updated and softened.

A refresh and refurbishment of existing gates could help alongside new soft landscape features. Alternatively, the existing boundary could be removed in full / part and replaced with tree planting, rain gardens and intermittent fixed seating and planters to create a clear plot boundary but also create a more welcoming interaction with the College grounds.

### 2) Parking and access

Car parking provision within the College will most likely continue to be required. However with the introduction of improved active travel accessibility to the College and parking provision nearby as part of streetscape enhancements, there may be an opportunity to assess and review car parking requirements within the plot and potentially create more cycle and bike parking closer to the entrance to the building.

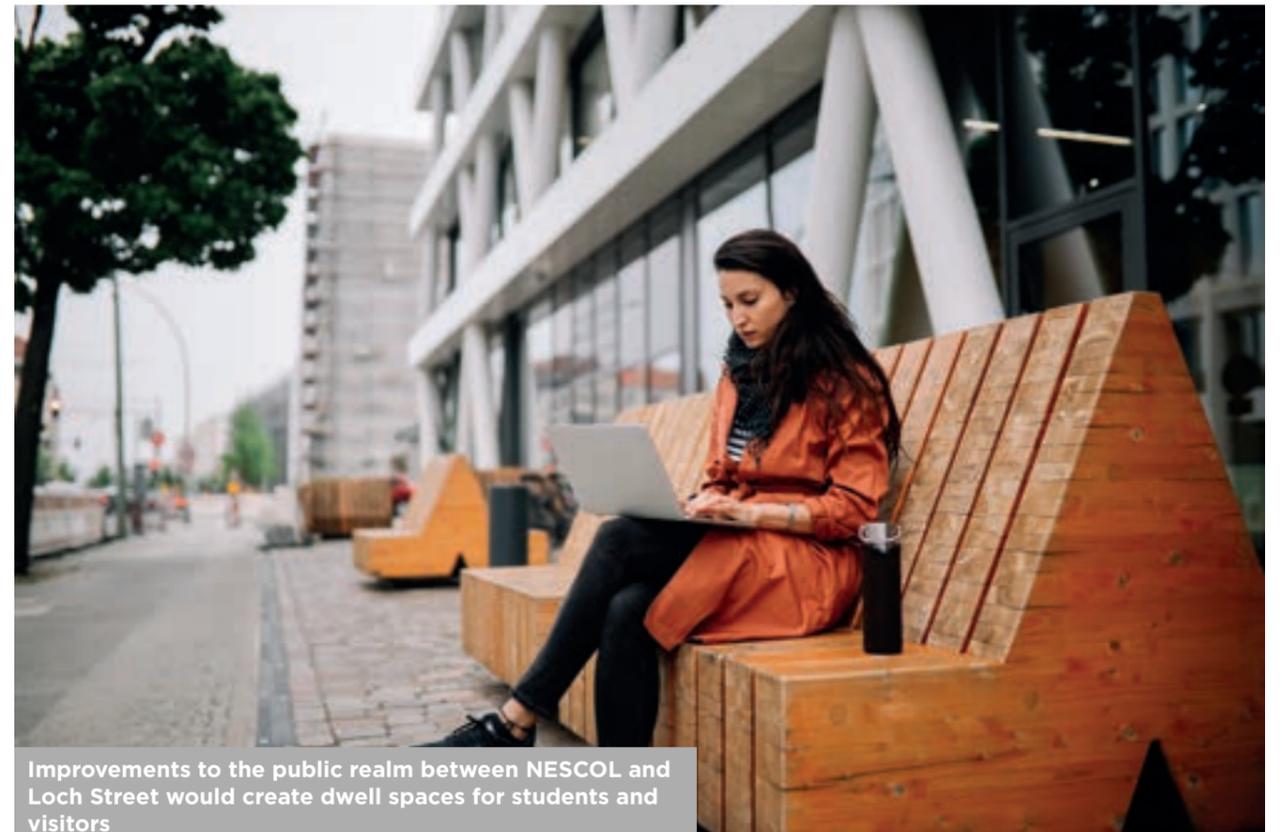
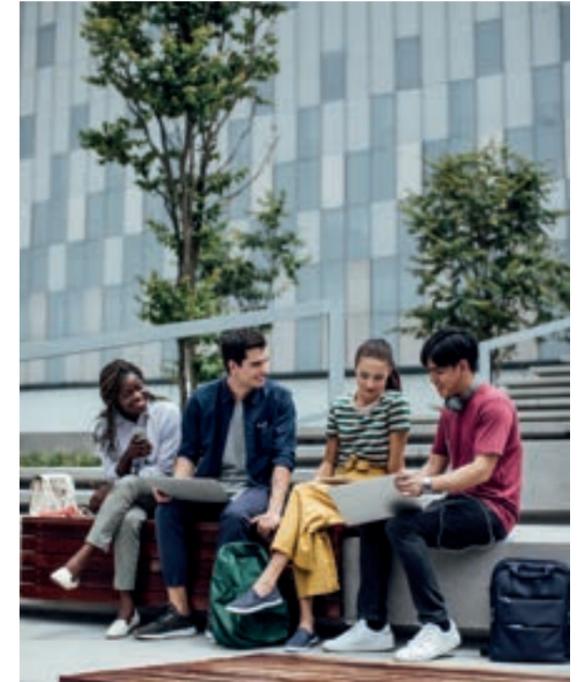
### 3) Enhanced arrival to the College

Opportunity to continue the arrival to the College from both Loch Street and Gallowgate by creating a more multi-functional space on approach to the building. Linked to a review of on plot parking, there may be opportunities to improve the arrival and use of the space as you approach from Loch Street. Introducing contemporary, comfortable seating to support outdoor learning and social meeting spaces for students. Creating clearer arrival points with directional signage, information points, cycle repair points, high speed outdoor secure wifi for students, and improved lighting that guides you to the entrance.

Each of these potential opportunities are identified as ways that the College could integrate more with its surrounds, by encouraging more dwell space and educational activities, within an environment defined by tree planting, new planting and places to meet.



The Masterplan seeks to better integrate NESCOL into the George Street area.



Improvements to the public realm between NESCOL and Loch Street would create dwell spaces for students and visitors

## Deliverability & Implementation

As the Vision, objectives and Transformational projects of the Masterplan are implemented, local stakeholders, the private sector and communities will be crucial to shaping the interventions emerging from this Masterplan.

It is recommended that dedicated working groups are set up to drive specific short term priority actions forward. The action focused working groups will be responsible for:

- Defining the details of the proposed action;
- Considering how it could be funded;
- Determining who needs to be involved in supporting its delivery and what their role will be;
- Seeking funding - this might require support from other partners;
- Determining the timetable for intervention;
- Monitoring and evaluating progress against key performance indicators and refining actions if agreed outcomes are not being met; and
- Reporting progress.

The makeup of the working groups will vary depending on the specific actions they are focused on. As such, the number of working groups and pace of delivery will depend on the number of stakeholder partners that can be engaged with and encouraged to get involved.

## On-going Communication & Engagement

The Masterplan has engaged a number of stakeholders and interested people throughout its development. On going engagement will be key to helping to continue to shape and drive the various projects outlined in the Masterplan. It will also be important to share and communicate on the following;

- Recent and proposed investment in the city centre
- Projects that are being worked up
- Funding that is being sought and accessed

- How people and stakeholders get involved.
- The key partners to be engaged with on a regular basis include:
- Public sector partners (Police, NHS, Fire, NESCOL)
  - Local businesses
  - New owners of Norco House and Bon Accord
  - Residents
  - Local community and interest groups.

Quality engagement should generate potential volunteers to get involved in the delivery of specific actions and potentially resources to support their delivery.

The following mechanisms should be used to engage with key partners:

- Enhancement of the community websites and strengthen profile on social media
- Public engagement consultations will be used to continue the dialogue with local residents as projects develop in detail
- 'Piggybacking Events' - for example a stall at markets showcasing what's on and how to get involved
- Existing and enhanced business networking events - to share emerging ideas, test support and encourage ideas to refine and support their delivery; and
- Identification of a team of local ambassadors who are kept up to date about what is going on in George Street and then they can showcase positive messages to their contacts. Ambassadors can be proactive local independent businesses leaders and other individuals who come into contact with lots of local people e.g. shop keepers.

## Next Steps

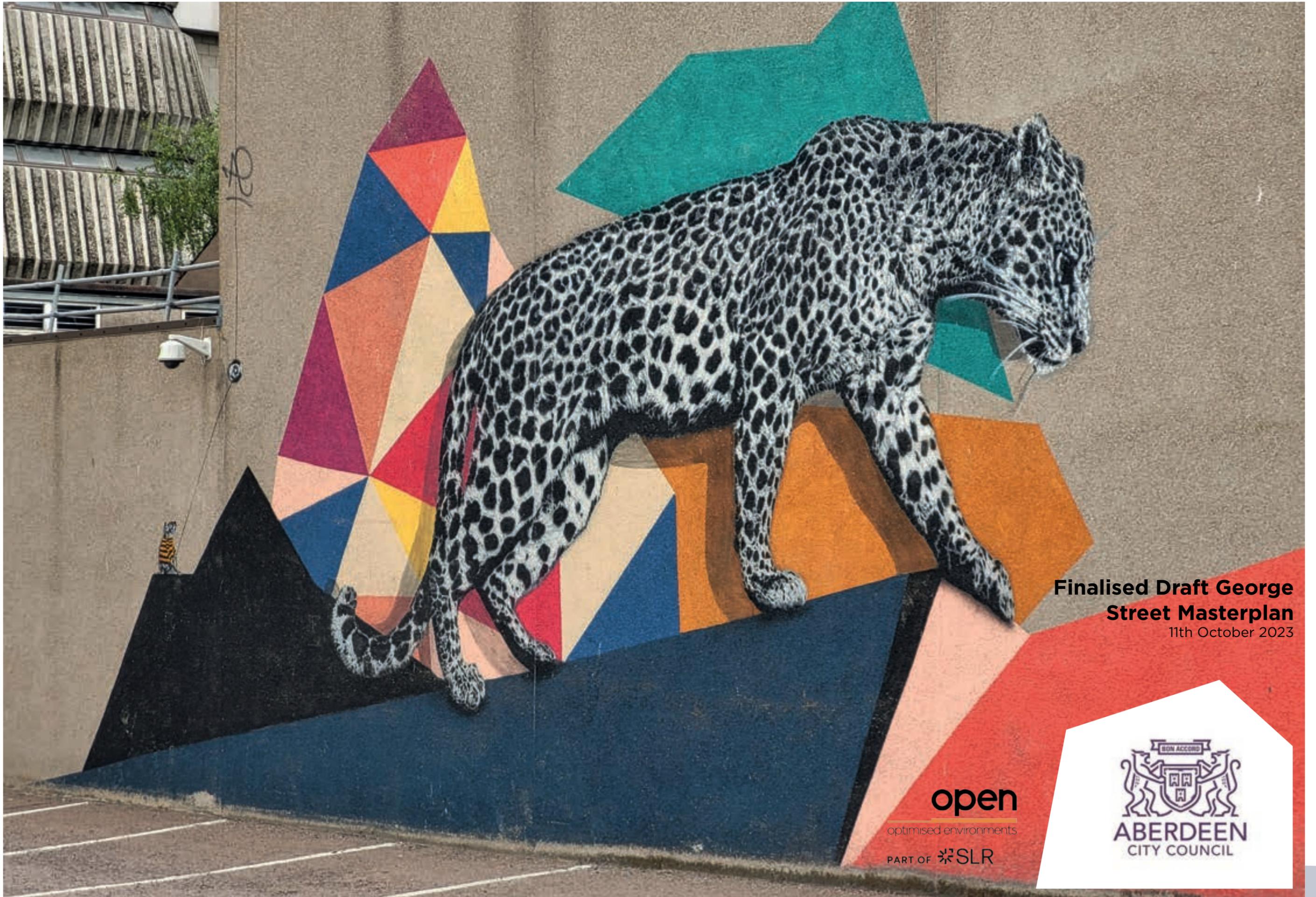
This Masterplan is just the first building block for regeneration of the George Street area and should be updated as a live programme of works to record progress on delivery of the projects within the Masterplan.

Further work will be carried out by the Council to understand and develop future opportunities, options and projects, aligned with the agreement of stakeholders.

Delivery of the projects identified in the Masterplan area will support and contribute towards delivering the wider objectives of the LOIP and City Centre and Beach Masterplan. They will be continually benchmarked and reviewed until completed.



Figure 85 - Illustrative concept of Harriet Street



**Finalised Draft George Street Masterplan**  
11th October 2023

**open**

optimised environments

PART OF  SLR



**Appendices**

# Appendix 1

## Projects Influenced by Consultation Feedback

The following table provides a more detailed overview of the potential future projects identified in Section 4 of the Masterplan document.

The projects identified will need further discussions with the local community, business and stakeholders to deliver. It should also be noted that the Council will support both the local community and local businesses where possible to bring these projects forward.

When	Project	Proposal	Where	Objective	Lead & Partners
Short Term (Quick Win)	<b>1. De-cluttering the Streets</b>	Removal of clutter & unnecessary signage, satellite dishes, pole signage etc	Focus on areas with proliferation of clutter e.g George Street and agree priority areas based on early implementation of public realm change to maximise positive impact.	<b>2</b>	ACC & Local Community (Partners to be identified)
Short Term		Residential front door enhancements - painted, restored, highlighted through public realm materials	For residential properties that open onto Local Access Streets and which have no front porch or threshold to the street	<b>2</b>	Local Residents Groups & ACC
Short Term (Quick Win)		De-clutter existing streetscape - combine highways and pedestrian signage, where possible and remove unnecessary items	Focus initially where public realm interventions are proposed and prioritise pedestrian focused environments e.g Lower George Street	<b>1, 2</b>	ACC
Short Term (Quick Win)	<b>2. Improved Street Appearance</b>	Deep clean of existing paving, street furniture and building façades	Areas to be identified by local community. Suggest focus on buildings where public realm interventions are proposed to maximise impact.	<b>1,2</b>	ACC, Aberdeen Inspired, Local Shop Owners
Short Term	<b>3. Identity</b>	Create an online neighbourhood presence	To cover the George Street Masterplan area as well as the wider George Street Community to the north to showcase events and create a place for residents, stakeholders and others to be kept up to date on related neighbourhood activity.	<b>1,2</b>	Community Organisations (to be identified)
Short Term	<b>4. Place Management</b>	Appoint local champions for George Street who will focus on delivering agreed Vision and strategies in the area e.g area promotion and assisting with delivery of meanwhile or temporary uses	To cover the George Street Masterplan area (and ensure co-ordination with adjacent community initiatives / projects)	<b>1,6,7</b>	Local Community Groups in partnership with ACC
Short Term		Establishing a core group of businesses owners / institutions to represent George Street and co-ordinate stakeholder activity	To cover the George Street Masterplan area (and ensure co-ordination with adjacent community initiatives / projects)	<b>1,6,7</b>	To be identified / connected if existing
Short Term	<b>5. Security</b>	Woman and Vulnerable Group Safety Audit	Area wide	<b>1,7</b>	Local Community with ACC
Short Term (Quick Win)	<b>6. Lighting</b>	Pop up street canopy / overhead lighting	Focused along George Street	<b>1,2</b>	Local community & arts groups with ACC & Aberdeen Inspired
Short Term		Front door lighting for residential tenements	Focus on residential properties that open onto public streets and which have no front porch or threshold to the street. A priority area could be George Street & Craigie Street	<b>1,2</b>	Local community / ACC / Aberdeen Inspired
Short Term (Quick Win)	<b>7. Reactivating alleys, lanes and back streets</b>	Making use of and activating underutilised spaces in a creative way e.g meanwhile uses, pop up events, temporary events space, tactical urbanism	Focus on streets, vacant spaces, car parks where there are low levels of activation.	<b>5,6,7</b>	Local Community Groups, George Street Businesses and ACC, Arts Groups, Outreach Groups
Short Term		Encourage meanwhile uses such as pop ups, street closures, parklets etc to improve activity	Area wide, beginning on George Street	<b>5,6,7</b>	ACC, Aberdeen Inspired, Local Shop Owners

When	Project	Proposal	Where	Objective	Lead & Partners
Short Term	<b>8. Creating an Adaptable Neighbourhood</b>	Using empty units for art exhibitions, book launches, College end of year exhibitions, temporary performance space, overspill exhibition space for the nearby Art Gallery, local street library etc	In areas vacant units that address primary or secondary streets to limit negative perceptions of void units. E.g Lower George Street and St. Andrew Street	<b>5,6,7</b>	Local Community Groups, Arts Groups, Educational Institutions, Bon Accord
Short Term	<b>9. A multi-generational space</b>	Catering for the diversity of visitors and residents, with improved facilities and amenities for various ages and abilities -e.g temporary seating, drinking water stations, shade areas, information points etc	Across areas with higher pedestrian footfall and near to local retail. Should be considered alongside parklets and spill out space being created.	<b>1,3,4</b>	ACC / Local Community
Short Term (Quick win)	<b>10. Art &amp; Colour</b>	Commission Creative placemaking work onto pedestrian areas & research the possibility of commissioning creative artwork in focussed areas to support NuArt profile in the Masterplan area	In pedestrian focused areas such as South George Street. Longer term this can be expanded to junction locations with improved pedestrian links	<b>2,6</b>	ACC, Other Art Institutions & NuArt
Short Term (Quick win)		Attractive visual screening across poor quality environments	Focus on areas where there are low levels of activation e.g. service yards / gable ends / blank elevation. Prioritise larger and most visible façades	<b>1,2,6</b>	Bon Accord Centre, shop owners, private landlords, ACC
Short Term		Street & pedestrian crossing colouring - super graphics from buildings to street level. This can be developed alongside NuArt locations to expand visual identity whilst respecting heritage setting and character	In pedestrian focused areas such as South George Street. Longer term this can be expanded to junction locations with improved pedestrian links	<b>2,4</b>	ACC & private landlords
Short term		Shop front creative interventions such as painted displays, painted shutters, window dressing etc.	Along streets where majority of shops close after 17:00. Focus along George Street, St. Andrew Street and John Street to deliver co-ordinated impact.	<b>1,2,6,7</b>	ACC, or a Creative and Cultural Organisation/ Institution, or Culture Aberdeen
Short Term	<b>11. Animating buildings / façades</b>	Research Creative Commissions for the façades of buildings in George Street	Build from Jopps Lane in partnership with willing land / property owners. Community can help to determine priority areas.	<b>1,2</b>	ACC, Creative and Cultural Organisation/Institution, or Culture Aberdeen
Short Term	<b>12. Signage &amp; Fronts</b>	Early dialogue with owners of Bon Accord Centre to discuss enhancements to the north and eastern façades of the shopping centre and car park	Focus on the northern and eastern elevations of the Bon Accord Shopping Centre	<b>6,7</b>	ACC / Bon Accord Centre Owners
Short Term	<b>13. Alive after 5pm</b>	Establishing a strategy to co-ordinate later opening times / weekends across various uses. E.g NESCOL& Robert Gordon's Colleges evening openings promoted to local restaurants, cafés, bars etc to encourage college patrons into the area.	Across the neighbourhood	<b>1,7</b>	ACC, Aberdeen Inspired, Local Shop Owners, Local Business Groups, NESCOL & Robert Gordon's College
		Introduce reward vouchers initiative etc to promote cross business patronage between retailers and other businesses as well as local residents' discounts.			
Short Term (Quick win)	<b>14. Calendar of events</b>	Curate a co-ordinated calendar of events for George Street area & promote through social media	Area wide	<b>1,6,7</b>	ACC, Aberdeen Inspired
Short Term		Celebrate heritage and the ornate architectural diversity through commercial events, walking tours, online/apps, heritage signage / info points and feature lighting.	Focusing on historic buildings of architectural interest and / or social and cultural significance.	<b>1,6,7</b>	ACC, Aberdeen Inspired

When	Project	Proposal	Where	Objective	Lead & Partners
Short Term (Quick win)	<b>15. Making streets work harder</b>	More flexible use of parking spaces - creating PARKlets which reclaim parking bays for socialising, play and dwell time. Create a PARK(ing) Day which could bring a festival of parklets to the area.	Focus where the type of existing businesses can benefit from adjacent spill out space / dwell space i.e cafes, bars, community uses	<b>4,5,6</b>	ACC
Short Term		Better provision and / or quality of cycle parking in convenient locations	Prioritise anchor locations such as Bon Accord Centre, NESCOL and George Street as well as at arrival gateways / bus stop locations.	<b>3,4</b>	ACC
Short Term (Quick win)		Experimental Traffic Regulation Order (ETRO) 1 - retain the 2-way operation of Berry Street but for car-park access/egress only	Berry Street, Loch Street and St. Andrew Street. (Note existing small central reservation exists along Berry Street which will need some minor amends to implement ETRO).	<b>4</b>	ACC
Short Term (Quick win)		ETRO 2 - Reduce & restrict speed of traffic by narrowing carriageway with temporary build-outs and signage. If successful restrict to local access only.	Charlotte Street between Spring Garden and John Street	<b>4</b>	ACC
Short Term (Quick win)		ETRO 3- Remove general traffic along Blackfriars St northbound, creating more freedom for pedestrians, cyclists and public transport.	Blackfriars Street	<b>4</b>	ACC
Short Term		Introduce bin storage or shelters where bins are required to be stored on street	Residential locations and pedestrian focused areas e.g Craigie Street & St. Andrew Street	<b>3,4</b>	ACC / Aberdeen Inspired
Short - Medium Term	<b>16. Creating an Adaptable Neighbourhood</b>	Tap into local talent by providing space for start-ups / pop up events to encourage students to stay beyond their course. Establishing partnerships and routes to wider institution involvement in George Street area events / initiatives	Adjacent to Bon Accord Centre. Any new urban spaces created within Neighbourhood	<b>7</b>	ACC, Aberdeen Inspired, Educational Institutions, Arts & Culture Institutions
Short - Medium Term	<b>17. Art &amp; Colour</b>	Commission a sculpture trail across the area	Area wide, beginning on George Street but route could be co-designed with community	<b>1,6</b>	ACC, Creative and Cultural Organisation/Institution, or Culture Aberdeen
Short - Medium Term	<b>18. Improved Street Appearance</b>	Improve quality & consistency of shop front signage and consider means to align with current policy and identify opportunities for shop owner support	Area wide, beginning on George Street	<b>6,7</b>	ACC, Aberdeen Inspired, Local Shop Owners
Short - Medium Term	<b>19. Increasing dwell time</b>	Coordinated palette of street furniture including provision for elderly, range of physical abilities, visually impaired, NESCOL students and cyclists	Area wide but focus firstly on John Street, St. Andrew Street, George Street and Loch Street	<b>4,6</b>	ACC, NESCOL
Short - Medium Term	<b>20. Smart High Street</b>	Visitor and residential information points - these could be pop up kiosks / interactive stands / live cycle counters / air quality readings / parking availability etc	Area wide, beginning on George Street and along cycle routes	<b>1,3,4,6</b>	ACC, Aberdeen Inspired
Short - Medium Term	<b>21. Provision of Necessities</b>	Consider provision of basic services, public toilets, 'changing spaces' toilets, and disabled access points	Along streets with pedestrian activity and along local access streets.	<b>1,4,5,6</b>	ACC
Medium Term	<b>22. Redefining Residential Streets - Urban Pilot Project</b>	Customising existing streets to support healthier and sustainable urban life for residents - interventions could include designated bin storage, defined parking bays, EV charge points, street greening / communal gardens, play street elements, urban allotments, lighting, SuDS etc	Focus on areas with density of residential accommodation eg. Charlotte Street and Craigie Street. Exact areas to be co-ordinated with community and developed in co-ordination with ETROs	<b>3,4,6</b>	Local residents supported by ACC
Medium term	<b>23. Street planting &amp; urban greening</b>	Installing green walls, SuDS, Living walls & roofscapes and other urban greening solutions as part of a neighbourhood wide strategy to improve bio-diversity	Focusing on large scale blank gable ends and façades that are orientated to support green walls. Secondary focus on smaller infill areas	<b>2,3</b>	ACC / Business Owners

When	Project	Proposal	Where	Objective	Lead & Partners
Medium Term	<b>24. Improved Street Appearance</b>	Develop a palette of materials and street furniture to help celebrate George Street character (to be developed alongside public realm detail design stages)	Across streets which are undergoing interventions to increase pedestrian space	<b>3,4,5</b>	ACC
Medium Term		Uplighting and feature lighting to illuminate buildings	Primary focus on buildings of interest including Norco House and other historic buildings	<b>1,6</b>	ACC / Business Owners / building owners
Medium Term		Create a signage and wayfinding strategy, with a new suite of bespoke and George Street characterised signage kit, including at key arrival points into the neighbourhood	Focus on edges of the neighbourhood where people travel into the area (gateway locations). Develop along main pedestrian routes that flow from those gateway locations.	<b>1,4,6</b>	ACC / Business Owners
Medium term	<b>25. Security</b>	Improved CCTV coverage	In areas where community and local businesses report most anti-social behaviour incidents	<b>6</b>	ACC?
Medium term	<b>26. Lighting</b>	Lighting strategy for George Street - for both wayfinding & security	Area wide as part of proposed lighting strategy	<b>4,6</b>	ACC
Medium term	<b>27. Street planting &amp; urban greening</b>	Street trees, planting and rain gardens as part of transformational project 3. Opportunities to introduce increase in bio-diversity, pollinator friendly planting and nature rich environments	Area wide as part of wider public realm strategy & transformational project 3	<b>4,6</b>	ACC
Medium term	<b>28. Making primary routes work harder</b>	Inclusion of cycle lanes along key routes	Along streets identified within the Masterplan cycle accessibility strategy	<b>4</b>	ACC
Medium - long term	<b>29. Improved Environmental Performance</b>	Retrofitted rain gardens and planting which filters out pollutants across existing buildings (roofs / façades) to combine with streetscape improvements. Programme could also be extended to existing internals of buildings for retrofit, improving thermal insulation and reduced energy consumption.	Area wide intervention across existing streets	<b>3,4</b>	ACC / National Govt, private occupiers, local businesses, local residents and other potential retrofit grants / funds
Medium - Long Term	<b>30. Improved Street function</b>	Street redesign to include: <ul style="list-style-type: none"> <li>• Carriageway narrowing</li> <li>• Footpath build out at junctions / junction improvements</li> <li>• Pedestrian prioritised crossing points</li> <li>• Footpath widening</li> <li>• On-carriageway bus stops to prioritise public transport and slow cars</li> <li>• Spill out space for businesses</li> <li>• Seating and rest points</li> <li>• Cycle parking and repair stations</li> </ul>	Area wide but focus firstly on John Street, St. Andrew Street, George Street and Gallowgate.	<b>3,4</b>	ACC
Medium - Long Term	<b>31. Prioritising Sustainable Travel</b>	Reconfiguring current bus routes as well as provision of enhanced bus stops with interactive boards / wifi / charging points to encourage greater use and improved passenger experience	John Street, St. Andrew Street, George Street and Loch Street (in liaison with bus operators).	<b>1,3,4</b>	ACC, bus operators
Medium - Long Term		Ensuring there is adequate space for cyclists along key routes with cycle parking through the area would encourage more cyclists to use the area safely.	Along streets identified as formalised and advisory cycle routes as per the Cycle Accessibility Strategy	<b>3,4</b>	ACC
Longer term	<b>32. Animating buildings / façades</b>	Create active uses and frontages along Loch Street at ground level.  Improve entrance to the George St approach by widening the openings to the building at ground floor	Loch Street and George Street	<b>1,4,6,7</b>	Bon Accord Centre

# Appendix 2

## Planning Context

### Planning Policy Review

#### Overview

This section sets out the policy and guidance at local administrative levels that have influenced preparation of the George Street Masterplan.

The policy sections of the Aberdeen City Centre and Beach Masterplan document provide further policy and guidance at UK, Scotland and Regional administrative levels.

#### City Centre Recovery Task Force: Report

Covid-19 has changed the face of city centres up and down the UK. As a result of necessary health measures, businesses and streets usually bustling with people were transformed overnight.

In 2021, the Scottish Government and Scotland's seven cities established a Task Force to identify the actions needed to take together to support city centre recovery from the effects of the pandemic, working through our existing successful partnership of the Scottish Cities Alliance.

The pandemic has heightened awareness of inequality, poverty and disadvantage, and there is clear evidence that harm has been felt unevenly. The Scottish Government has set the national direction with its ten-year National Strategy for Economic Transformation, which will build on the Covid Recovery Strategy to create a greener, fairer, more inclusive wellbeing economy.

The key messages arising from the report were about the nature and role of city centres and how, without people, city centres are just empty spaces and buildings. What makes our cities flourishing, vibrant, social, creative, interesting and resilient is that they are places where large numbers of people come together.

The principal challenge considered in the Task Force Report is how to attract people back into our city centres, and this is something that should be applied when looking at the future of George Street. How to attract people back into George Street.

#### Local Policy

At the Local level, planning policy guidance and advice is provided by the Aberdeen Local Development Plan 2023. The plan provides detailed planning policy to be considered when assessing proposals across the city, including the city centre and beach; and also provide more detailed information and briefs in relation to specific opportunities (OP sites).

The Council's **Local Outcome Improvement Plan (LOIP)** and associated Locality Plans are key in setting out a Vision for Aberdeen as "a place where all people can prosper", and considers the People, Place and Economy of Aberdeen. The refreshed Local Outcome Improvement Plan was approved in July 2021, with an annual report published in July 2022. **The LOIP identifies 15 'stretch' outcomes to be delivered by 2026 and 75 shorter term improvement aims.** Stretch Outcomes particularly relevant to George Street include: **Child Friendly City** which supports all children to prosper and engage actively with their communities by 2026; **Healthy life expectancy** (time lived in good health) is five years longer by 2026 - this includes the Key Drivers of encouraging adoption of healthier lifestyles, and increasing satisfaction and use of community facilities and green environment to increase the health and well-being for older people and people managing long term conditions); **Addressing climate change** by reducing Aberdeen's carbon emissions by 61% by 2026 and adapting to the impacts of our changing climate; and, **Increasing sustainable travel** with 38% of people walking and 5% of people cycling as main mode of travel by 2026.

### Local Planning Advice

#### Aberdeen Local Development Plan 2023

The repair and replacement of shops and signs is a prevalent topic. Historic and original features add to the overall pleasant experience of buildings, streetscape and their wider surroundings. Inappropriate design, proportions or materials can detract from a building and the wider streetscape and setting.

The Supplementary Guidance acknowledges a principle of retaining and repairing historic materials, with replacement requiring thorough consideration. New shops and signs need to be designed for their location to ensure they provide a positive experience to the streetscene.

The guidance applies to existing and new shop fronts and signs. The guidance is to be used on a citywide basis for retail and commercial premises including banks, betting offices, restaurants, estate agents and beyond. The signage guidance should be considered in all instances.

#### Hierarchy of Centres

Scottish Planning Policy states that a sequential approach should be used when selecting locations for all uses which generate significant footfall, including retail and commercial leisure uses, offices, community and cultural facilities, and where appropriate, public buildings; and requires that locations are considered through a hierarchy of centres. Within this SPP, George Street is identified as a Neighbourhood Centre which is the preferred location for local retail, commercial leisure, community and cultural provision only.

#### Transport and Accessibility

Transport provision should be considered from the very outset of a development proposal, with particular attention paid to the need to encourage sustainable travel.

The Local Transport Strategy highlights the role that spatial planning can play in ensuring Aberdeen has "a sustainable transport system that is fit for the 21st Century, accessible to all, supports a vibrant economy, facilitates healthy living and minimises the impact on our environment" through the careful siting and design of development and the implementation of parking policies and Travel Plans to discourage unnecessary car travel.

A sustainable transport system will also help reduce emissions to tackle climate change, improve air quality, and lead to a healthier population.

## Green Space Network and Open Space

Access to good quality green and blue infrastructure, including areas of open space and green networks, will contribute to a greener, healthier, smarter, safer, stronger, wealthier and fairer city. This guidance will assist the George Street Masterplan in assessing and demonstrating their compliance in enhancing and improving green infrastructure to create useful, sustainable and well used places. This will benefit the wider community, the natural environment and support a healthy economy.

## Big Buildings

Guidance is given to ensure big building proposals, when appropriate, will have a positive impact on the streetscene and surrounding area, and Aberdeen's skyline.

Big buildings can be tall buildings and/or bulky buildings. Generally a big building is regarded as one that exceeds the general height of the surrounding built context and/or whose footprint is in excess of the established development pattern of the surrounding area.

According to guidance, big buildings should:

- Complement or improve the existing site context, consider the natural topography, scale and height of structures, urban grain, streetscape and built form, open spaces, effect on the skyline, and respect, reinforce and, where applicable create opportunities for enhanced views and vistas and make a positive contribution to the skyline when viewed from all angles from near, middle and distant views, both during the day and at night;
- Consider the environment and amenity of those in the vicinity of the building, including micro climate, overshadowing, glare reduction, night time appearance, internal and external lighting;

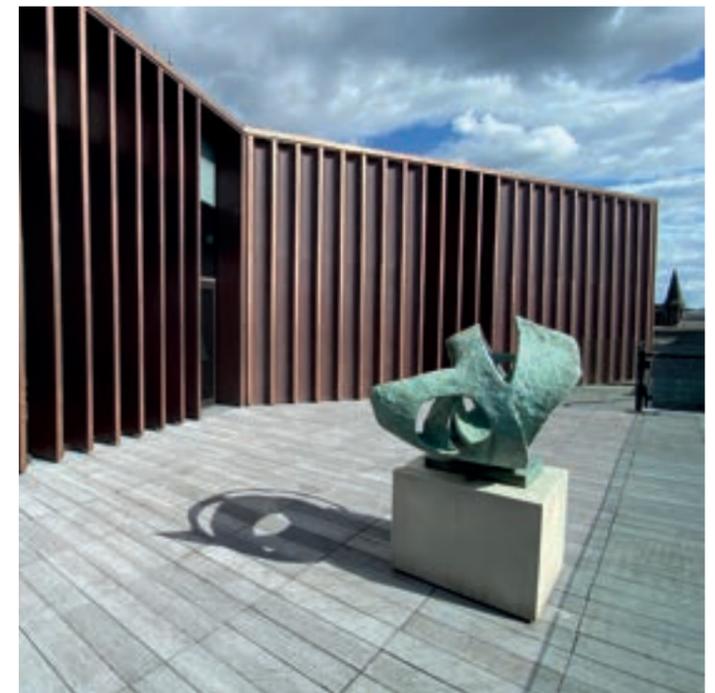
- At street level, present an active frontage and be comfortable for people and, where feasible, include publicly accessible areas within the building;
- Be situated in areas where existing topography, urban scale and transport make them sustainable;
- Have a well-designed vertical emphasis;
- Be constructed of high quality materials, craftsmanship and have detailing with low maintenance implications;
- Provide fully integrated servicing arrangements which are, wherever possible, off street;
- Comply with civil aviation requirements and those of other telecommunication, television and radio transmission networks; be adaptable over time and utilise best sustainable practices; and
- Contain a mix of uses rather than rely on a single use to achieve a viable development. It may be appropriate for big buildings to form part of a wider development mix with smaller scale buildings that reduce any dominating impact.

## Urban Realm Manual 2021 (Draft)

The Draft Urban Realm Manual 2021 aims to provide clarity, certainty and consistency on the city's requirements for a comprehensive approach to understanding and developing place and movement hierarchies, their appropriate hard landscaping material specifications, the retention and re-use of important features as well as identifying new opportunities to sustainably enhance and green the city.

Maintaining and enhancing the existing built environment, as well as delivering new development of distinctive high-quality places, is important to Aberdeen. To consistently have sustainable and efficient streets and places that are easy for all to use, that look good and have longevity give a positive impression and a lasting sense of place are fundamental to successful city life.

The public realm of George Street and the surrounding streets is something to be enhanced throughout this Masterplanning process, so by considering the principles of the urban realm manual and responding to the place and movement context in planning, design, responsible sourcing, responsible use and the principles of the circular economy, a successful place can be achieved.



## City Centre and Beach Masterplan Projects

As outlined in the City Centre and Beach Masterplan, there is a concentration of growth across the city of Aberdeen, which is prioritising people focused development, with an emphasis that supporting infrastructure and public spaces are integrated.

An emerging development context is changing the way the City will function over the course of the next few decades, through a suite of committed projects and those that are already well under construction.

Connectivity is an important consideration, with increased connections promoted through the area and to other key points in the city centre by foot, cycle and public transport.

Within the Masterplan study area, at the time of writing, both the former John Lewis store (Norco House) and the Bon Accord Centre have potential to influence the future regeneration of the neighbourhood. The Bon Accord Centre has an active planning consent for proposals along George Street and Norco House has new owners.

1. Some of the immediately significant pipeline development and infrastructure projects which will influence the George Street Masterplan beyond its boundary include; Upperkirkgate and Schoolhill Streetscape Improvements which look to pedestrians sections of the street as well as improve public realm quality along this corridor.
2. Improved public realm setting to the Aberdeen Art Gallery

3. A new urban park in Union Terrace Gardens
4. Broadford Works proposing to deliver c.890 units as part of a mixed use development, incorporating conversion of Listed Buildings.

Moving further out from the immediate city centre, some other developments of influence include:

1. Aberdeen Beach and associated developments
2. Aberdeen Stadium
3. Beach Boulevard

Also the new Beach Development Framework North and South have been identified on the plan adjacent.

**Aberdeen City Council's role in the future regeneration of George Street will be one that supports growth and change, using its powers to help facilitate and co-ordinate the appropriate type of change in line with the George Street Vision and objectives. The Council welcomes the opportunity to engage with and work collaboratively with stakeholders and interested parties.**



Indicative view of north Belmont street status: detailed design and delivery in conjunction with local stakeholders



Schoolhill + Upperkirkgate  
Birds eye view from Aberdeen Art Gallery



Beach Masterplan  
Birds eye view



Aberdeen City Council's Planned Development Pipeline

### Aberdeen City Council Projects

- |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <ol style="list-style-type: none"> <li>1. Leisure facility</li> <li>2. Beach ballroom</li> <li>3. Slipway</li> <li>4. Boardwalk</li> <li>5. Beach pavilion building</li> <li>6. Broad Hill structures</li> <li>7. Beach Boulevard</li> <li>8. Guild Street</li> <li>9. Rose / Thistle / Chapel Street</li> <li>10. Upperkirkgate / Schoolhill</li> <li>11. Castlegate</li> <li>12. Urban park</li> <li>13. Events park</li> <li>14. Landscaping</li> <li>15. Broad Hill</li> <li>16. Reconfigured beach landscaping</li> <li>17. Beach ballroom plaza</li> <li>18. Beach village</li> <li>19. Justice street roundabout</li> </ol> | <ol style="list-style-type: none"> <li>20. Berryden Improvements</li> <li>21. Aberdeen Market</li> <li>22. Queen Street</li> <li>23. Union Terrace Gardens</li> <li>24. Union Street Public Realm</li> <li>25. Aberdeen Market Public Realm</li> </ol> <p><b>Privately Led Projects</b></p> <ol style="list-style-type: none"> <li>1. North Dee Business Quarter</li> <li>2. Mither Kirk</li> <li>3. Aberdeen Broadford Works</li> <li>4. Woolmanhill Former Hospital</li> <li>5. Bon Accord Centre</li> <li>6. St Nicholas Centre</li> <li>7. Trinity Centre</li> <li>8. Union Square</li> <li>9. Stadium</li> </ol> |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

# Appendix 3

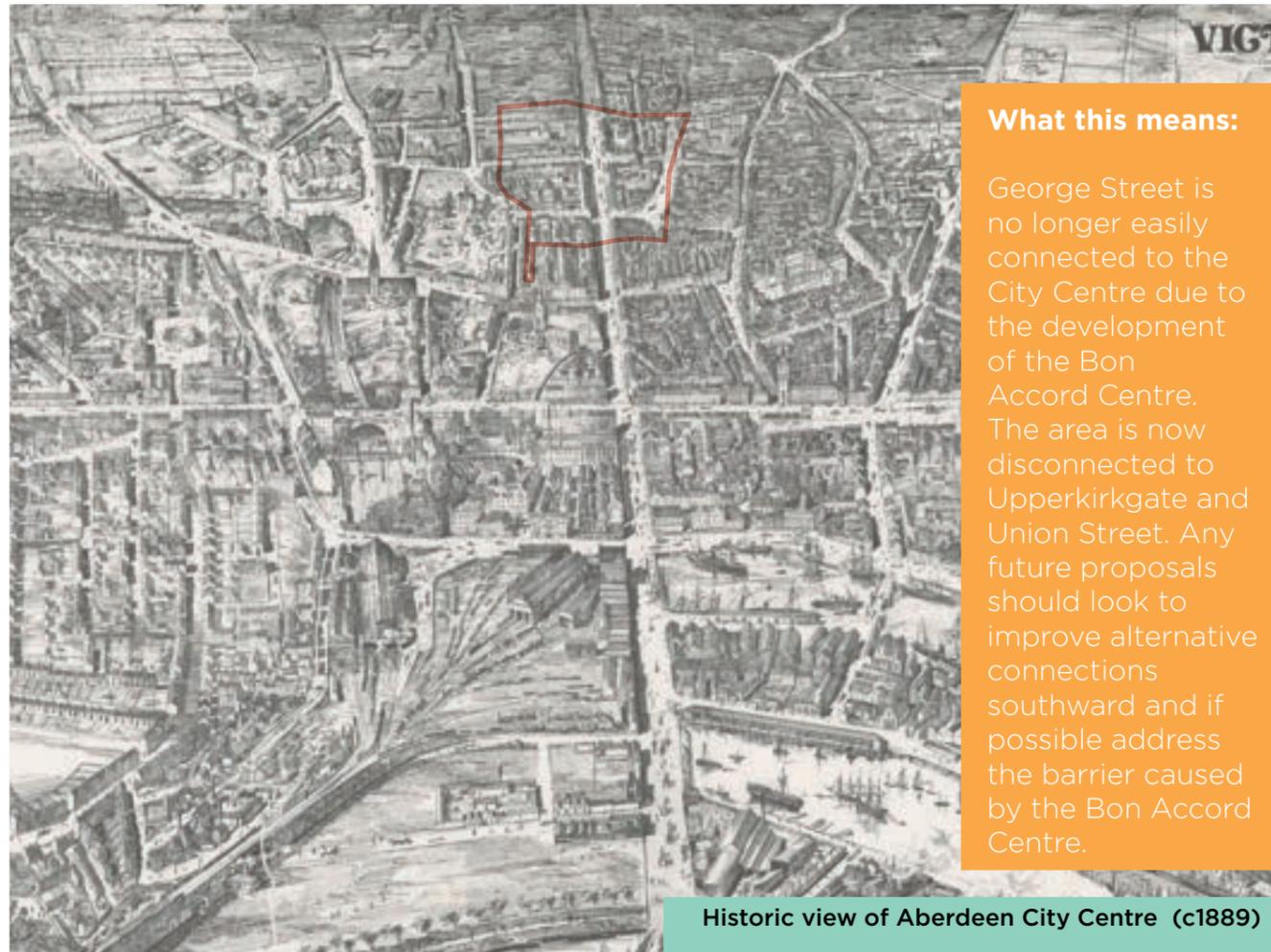
## Spatial Context

### Respecting Conservation And Heritage

The George Street area has a long standing, dense urban character, with most of its historical urban grid remaining intact today. George Street once formed a powerful urban corridor, which ran parallel to King Street, leading from the City's docklands, across Union Street and northwards out of the city centre towards Kittybrewster where local farm marts and agricultural trade was located. George Street was further reinforced as a key corridor in the early to mid 1900's, carrying a tramway along its length.

Over the years, the urban context of George Street has dramatically changed with the construction of the Norco "Ziggurat" building, and later the development of the Bon Accord and St Nicholas centres across the southern end of George Street.

The historic mapping on this page reveal the original nature and character of George Street. Much of current thinking in urban planning seeks to reinforce and identify historic patterns and character to create a strong sense of "embedded place."



#### What this means:

George Street is no longer easily connected to the City Centre due to the development of the Bon Accord Centre. The area is now disconnected to Upperkirkgate and Union Street. Any future proposals should look to improve alternative connections southward and if possible address the barrier caused by the Bon Accord Centre.



A dense work community with Candle & Soap Works, Stables, Public Baths and Foundries. George Street provides a direct connection through to Union Street and further south to the docks.



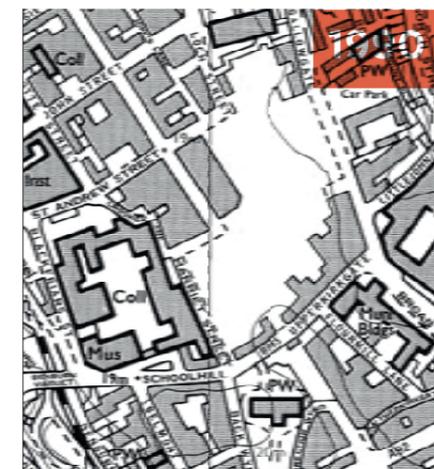
Introduction of the tramway along George Street which connected Kittybrewster train station to the tramway along Union Street to the south. An important neighbourhood through which activity and people moved.



Some of the denser historic grain becomes eroded with new development and infill. Robert Gordon's College also sees redevelopment. A central tramway through George Street has been discontinued.



Relatively little change through to the 70's although Loch Street continues to function as an important secondary street to George Street providing access to local schools, Places of Worship and Public Offices.



By 1990- 1995 both the George Street and Loch Street connection to Upperkirkgate and further south to Union Street are severed, with the area cleared to make way for the Bon Accord Centre Development.



By 2022 the area has undergone significant change to the south and east of the study area. Most of the historic grain remains intact along George Street, St. Andrew Street, John Street & Craigie Street.

# Listed Buildings

The George Street area sits just north of the City Centre Conservation Area, with Harriet Street and Gallowgate connecting into the study area.

There are a number of listed buildings / structures surrounding the George Street area and a small number within including:

## Within study area

- Category B listed 230 George Street And 34 John Street (3-storey, 4-bay Classical Bank);
- Category B listed 261-265 (Odd) George Street (former college building with prominent octagonal tower);
- Category C listed 46 Charlotte Street and Arched Gateway;
- Category C listed 26 John Street (3-bay warehouse with ornate Dutch gable to John Street elevation);
- Category C listed 119-127 (Odd Nos) George Street (5-bay commercial and residential building with distinctive wallhead stack and flanking crow-step gables); and
- Category C listed 111 Gallowgate including Boundary Wall and Railing.

## Beyond study area

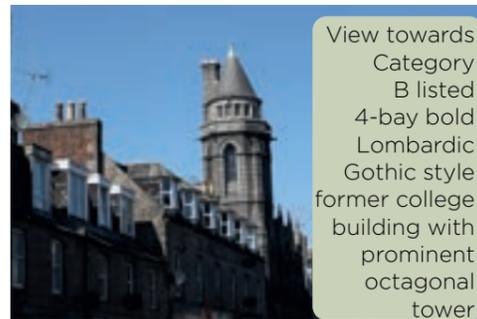
- A number of Category B & C listed buildings along Upperkirkgate;
- Category A listed Porthill Court Residential tower blocks;
- Category A listed Robert Gordon College;
- Category B listed Gerrard Street Baptist Church (former united free church)

There are a number of buildings identified through the baseline analysis which also add to the overall character and identity of the place, which are

identified in the plan adjacent. These buildings have no formal protected designation. The purpose of identifying these is to understand their relationship to George Street and is not intended as a recommendation for formal listing or categorisation.



Category A listed 1959 modern brutalist block of flats along Gallowgate



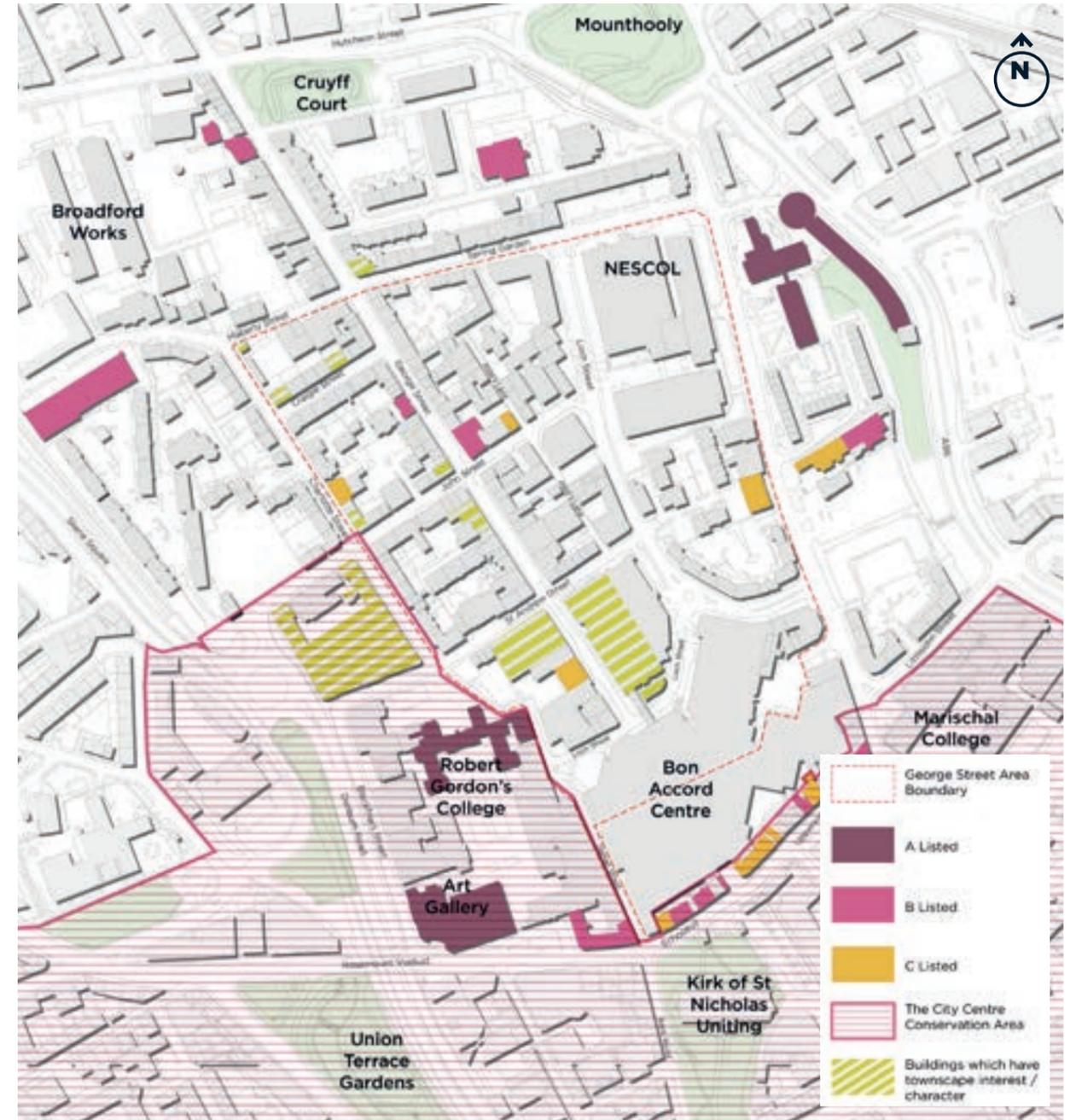
View towards Category B listed 4-bay bold Lombardic Gothic style former college building with prominent octagonal tower



View towards 1960s Norco House expressing its brutalist ziggurat facade.



View towards Category B listed classical bank building occupying a prominent George Street junction.



Historical Character

### Norco Building

Northern Co-operative Society's Norco House (most recently occupied by John Lewis) was built in 1966-70 for the (now-defunct) Northern Co-operative Society.

Designed by Covell Matthews Architects, the building is an example of brutalist, modernist architecture and intended to be a showpiece within the city's architectural make-up. The rippling, mound like profile of the massing of the building was particularly striking and unlike anything within the George Street area. The exposed concrete strips take similar design language from other well known brutalist buildings e.g Preston Bus Station as well as examples from other department store façades in Europe.

The building was taken over by John Lewis and renovated and converted in 1987. The department store was the organisation's northernmost store and was further expanded to the east in 1987 as part of the expansion of the John Lewis Partnership into Aberdeen. John Lewis has since closed (2021) and is now used by the NHS as a temporary vaccination hub on the ground floor.



### Loch Street Soup Kitchen

Aberdeen Public Soup Kitchen was first established as a charity in 1800 in St Mary's Chapel in St Nicholas Church. It supplied a breakfast of coffee and bread and a lunch of bread, soup and a piece of beef or mutton to the needy of the city.

The Soup Kitchen moved to Loch Street in 1838 and this new building was opened by Lord Provost Stewart in December 1894. The ground floor dining room had accommodation for at least 50 people and the tables were marble topped. In 1926, a total of nearly 27,000 meals were provided with funding still coming from charitable donations since few could afford even the two pence for the lunch.

The building survived major developments in the surrounding area but in recent years its charitable purpose was no longer thought necessary, and it was converted into a café in the mid 1990s. In 2022, the building still stands at 41-43 Loch Street, with the covered walkway for the Bon Accord Centre to its south. It is occupied by a outlet of the Jo Malone London fragrance and candle store.



# Street Furniture

As demonstrated in the plan, there is a lack of seating and dwell space within the George Street area. Contrastingly, issues with bin storage are visible across the entire site. A clear demand for cycle storage presents an opportunity to expand existing cycle facilities.



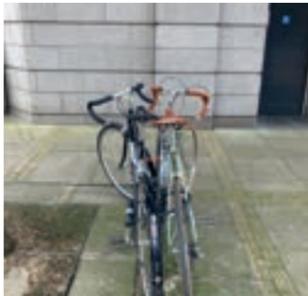
**Residential Bin storage** 

There are bins which spill out into the public realm across the wider George Street area. As well as lowering the visual appeal of a space, these also pose accessibility issues.



**Providing dwell space** 

There is a low provision of seating within the George Street area. Existing seating environments could be improved. For instance, whilst there are benches on George Street, these are positioned with their backs to the road and next to bollards. This makes for an unwelcoming dwell space.



**Cycle storage** 

Cycle storage is located adjacent to the college and also along George Street itself. This is well used and there is clearly a demand for adequate cycle storage and repair stations. There is an opportunity to incorporate additional facilities for cyclists across the area.

**What this means:**  
 Additional seating elements are required across the George Street area to improve accessibility and to create areas for dwell space. Bin storage is a problem across the entire site and negatively impacts visual quality and accessibility of the streetscape.



# Shop Fronts & Clutter

George Street is home to many independent and varied local businesses as well as regional and national retailers, each with their own graphic identity and approach to shop frontage promotion. As a result, the quality of the overall townscape of George Street can become cluttered and visually disjointed.

There are advantages to creating a more considered and co-ordinated approach to shop frontage appearance. Achieving more consistency with a broad set of guides can help to still promote local businesses and retailers whilst also ensuring that the architectural and visual aesthetic of George Street is able to be seen.

The images to the right illustrate some of the issues and how they could be overcome.

Opportunities to co-ordinate the design and aesthetic of shop frontages should be explored, in accordance with the Council's Shop Frontage Design Guidance.



**Inconsistencies in George Street's shop frontage**



**Indicative approaches to creating a more coherent townscape and retail environment**